

FED.ROAD DIST.NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	99C-02-00	2001	29	51

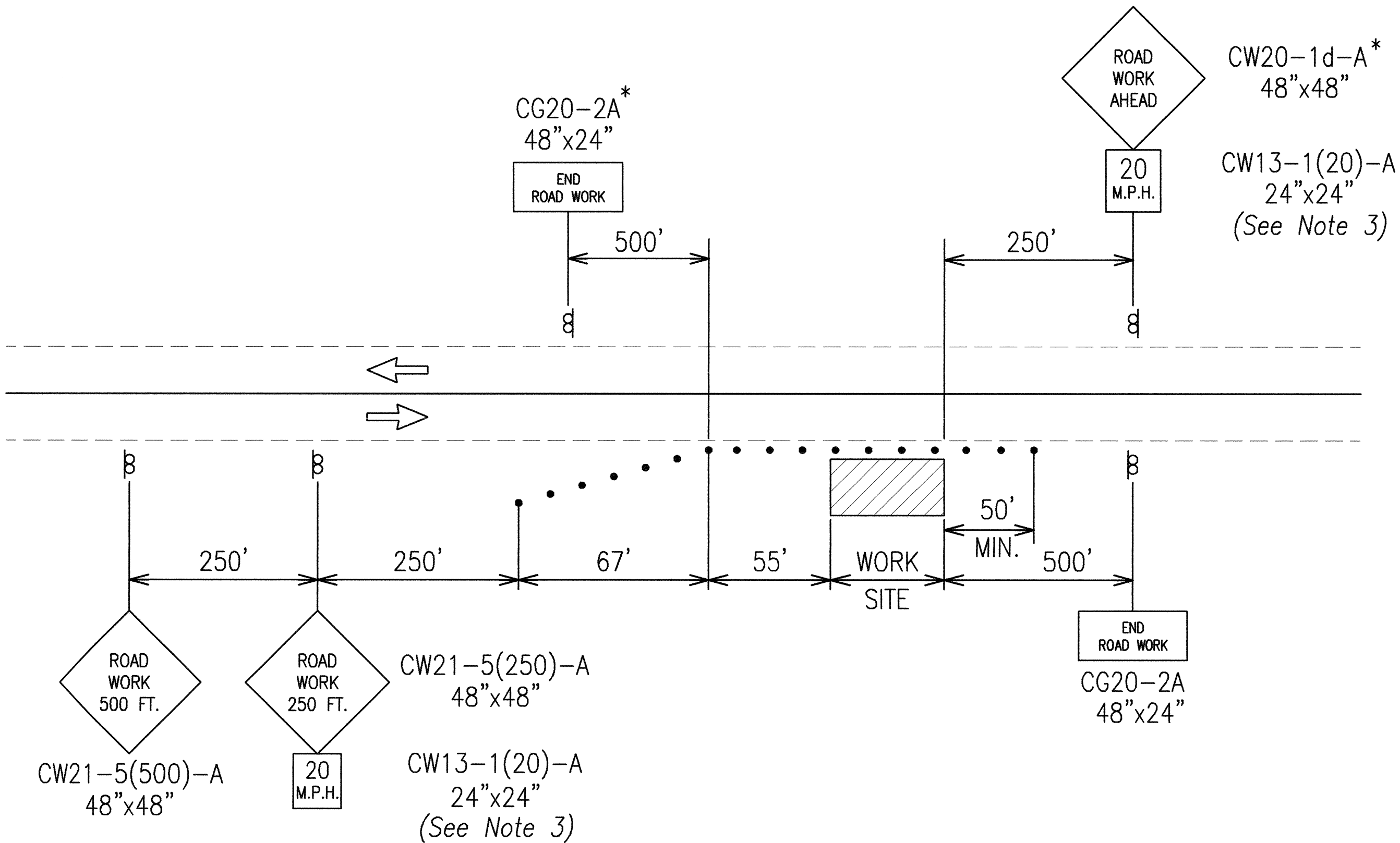
GENERAL NOTES
FOR TRAFFIC CONTROL PLAN

1. The Contractor shall make minor adjustments at intersections, driveways, bridges, structures, etc. to fit field conditions.
2. Cones or delineators shall be extended to a point where they are visible to approaching traffic.
3. Traffic control devices shall be installed such that the sign or device furthest from the work area is placed first. The others shall then be placed progressively toward the work area.
4. Regulatory and warning signs within the construction zone that are in conflict with the traffic control plans shall be removed or covered. All signs shall be restored upon completion of work.
5. Flagers and/or Police Officers shall be in sight of each other or in direct communication at all times.
6. When required by the issuing office, the Contractor shall install a flashing arrow signal as shown on the traffic control plans.
7. All traffic lanes shall be a minimum of ten (10) feet wide.
8. All construction warning signs shall be promptly removed or covered whenever the message is not applicable or not in use.
9. The backs of all signs used in traffic control shall be appropriately covered to preclude the display of inapplicable sign messages (i.e. when signs have messages on both faces).
10. As soon as the day's work is completed, the Contractor shall remove all traffic control devices no longer needed to permit free and safe passage of public traffic. Removal shall be in the reverse order of installation. Exisiting faded or obliterated pavement markings that are necessary for safe traffic flow in the construction area shall be replaced with temporary or permanent markings before opening the roadway to public traffic each day.
11. Permanent pavement markings and traffic signs shall be replaced upon completion of each phase of work.
12. Sign spacing and taper lengths shall be as shown on the traffic control plans.
13. Cones and delineators shall be spaced at a maximum of twenty (20) feet apart. A minimum of six (6) channelizing devices shall be used for each taper length.
14. The Contractor shall control traffic going in and out of driveways.
15. Buffer and taper areas on approach to any worksite shall be kept clear of vehicles and equipment.
16. Payment for traffic control shall be incidental to the various contract items.

TABLE 1 ADDITIONAL DIMENSIONS FOR TRAFFIC CONTROL PLANS							
POSTED SPEED LIMIT (M.P.H.)	SIGN SPACING (D) (FEET)	TAPER LENGTH (T) (FEET)		LONGITUDINAL BUFFER SPACE (B) (FEET)	SPACING OF CONES OR DELINEATORS (FEET)		
		W = 12' OR LESS ⁽¹⁾	W = GREATER THAN 12' ⁽¹⁾		TAPER	TANGENT	WORK AREA
20	250	200	W x 17	35	20	20	10
25	250	200	W x 17	55	25	25	10
30	250	250	W x 20	85	30	30	10
35	250	250	W x 20	120	35	35	10
40	500	350	W x 30	170	40	40	10
45	500	550	W x 45	220	45	45	10
50	1000	600	W x 50	280	50	50	10
55	1000	700	W x 55	335	55	55	10

NOTES:

⁽¹⁾ W = WIDTH OF LANE, SHOULDER, OR OFFSET



POSTED SPEED LIMIT 25 MPH
WORKING ON SHOULDER OR ROADSIDE
NOT TO SCALE

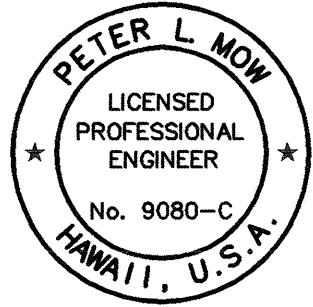
NOTES:

1. This plan may be used only if all road work, including equipment and workers are off the travel way.
2. Sign spacing length (D) and cone or delineator spacing are shown in Table 1.
3. Engineer will determine if Advisory Speed Sign is necessary.
4. Contractor will provide necessary adjustments to accomodate bicyclists and pedestrians.
5. CW20-1d-A sign required for posted speed limit of 45 M.P.H. or greater.
6. Signing is not required if the work area, including equipment and workers, is outside the Clear Zone Distance, as specified in the latest AASHTO Roadside Design Guide.

LEGEND

- Cone or Delineator
- ➡ Direction of Traffic
- ⊥ Sign
- ◇ Orange Flag

* Not required for opposing traffic
on divided or multi-lane highways.



THIS WORK WAS PREPARED BY
ME OR UNDER MY SUPERVISION.
Peter L. McGowan
ENGINEERS SURVEYORS HAWAII, INC.

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

TRAFFIC CONTROL PLANS

WILIKINA DR INTERSECTION IMPROVEMENTS
AT McNAIR GATE
Project No. 99C-02-00

Scale: No Scale Date: Sep 2000

SHEET No. 1 OF 2 SHEETS

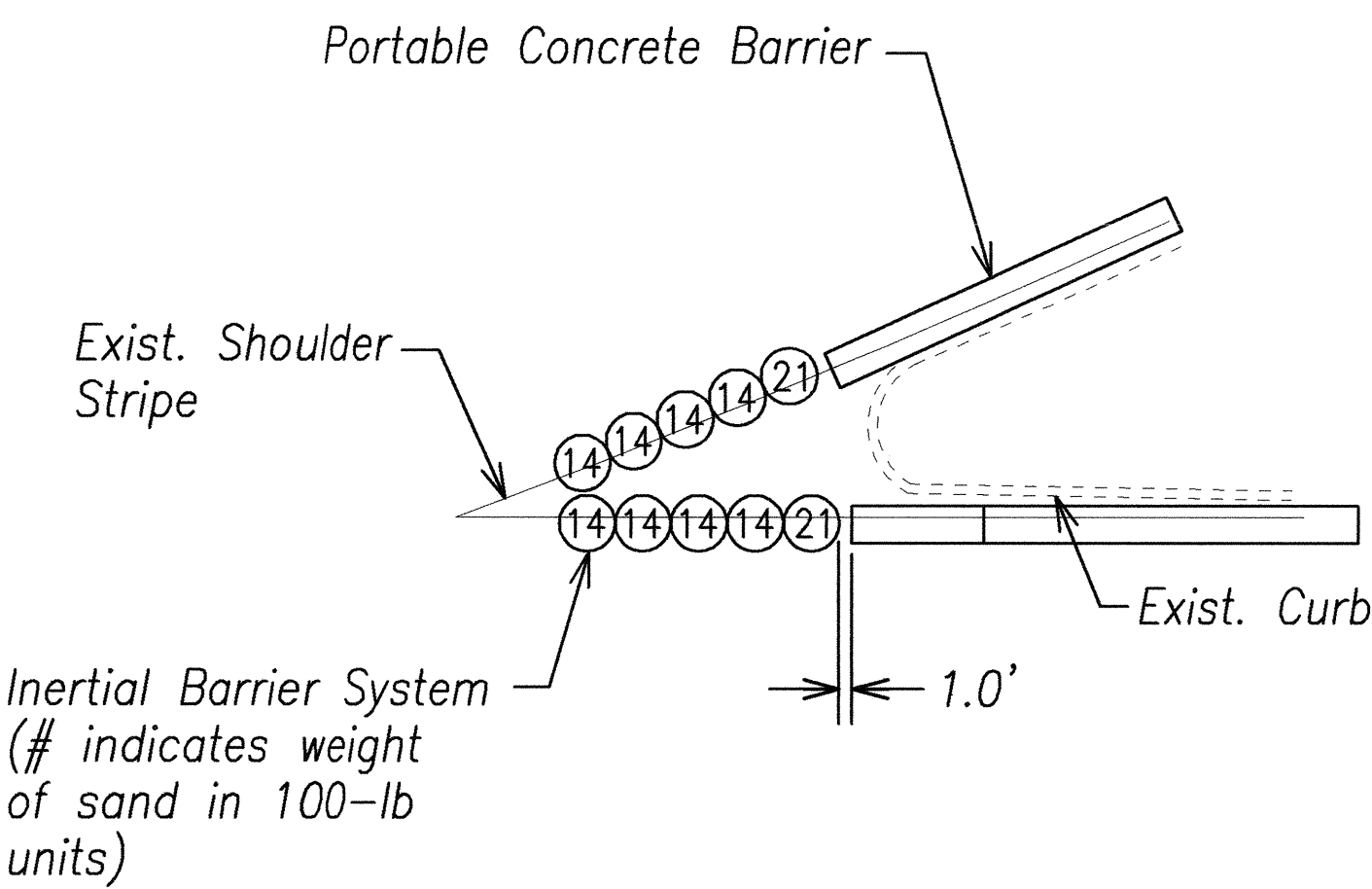
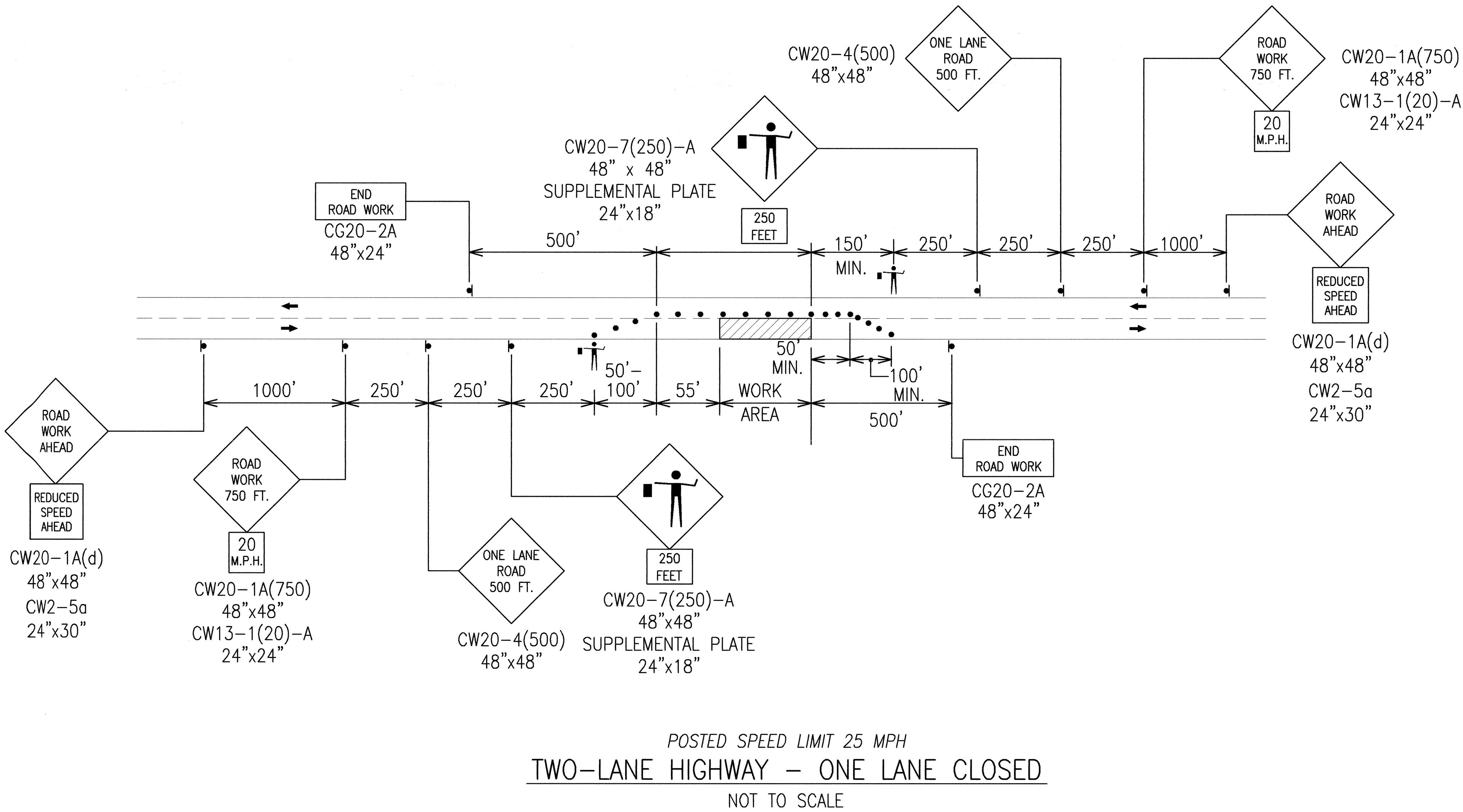
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NOTES:

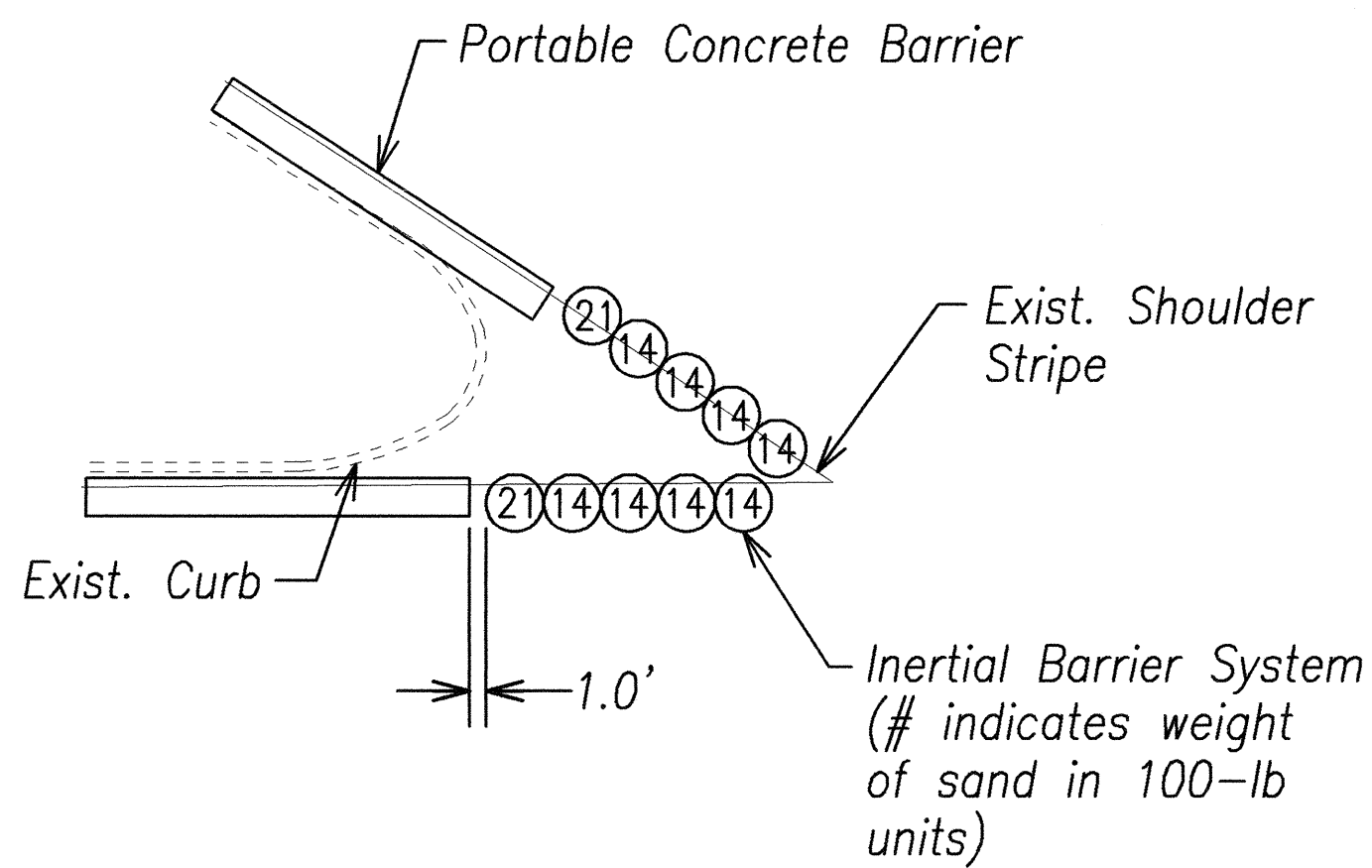
- CW20-1A(D) sign required for posted speed limit of 45 M.P.H. or greater.
- See Table 1, on Sht. 29, for additional dimensions.
- One lane road (CW20-4) and flagger ahead (CW20-7) signs shall be removed or covered when no work is being performed and lane is not closed.
- The advisory speed (XX) shall be determined by the engineer.

LEGEND

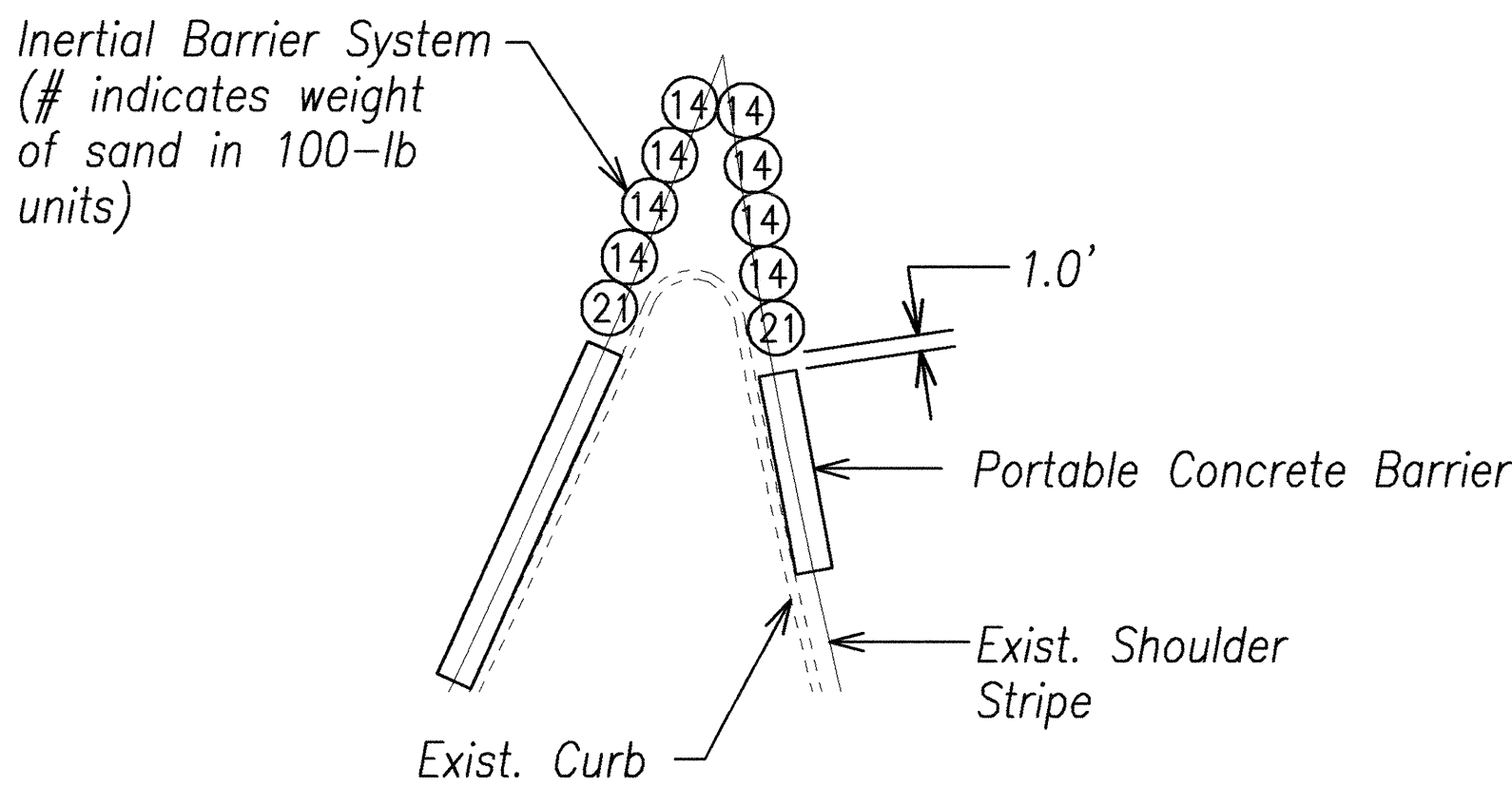
- SIGN
- CONE OR DELINEATOR
- DIRECTION OF TRAFFIC
- POLICE OFFICER/FLAGMAN
- WORK AREA
- FLASHING ARROW SIGNAL



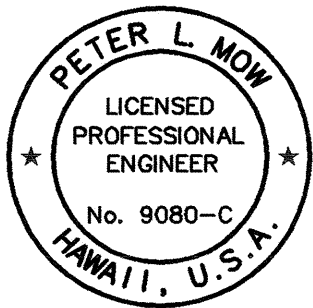
DETAIL-INERTIAL BARRIER SYSTEM
Wilikina Drive Sta. 11+42± (o/s 11' LT)



DETAIL-INERTIAL BARRIER SYSTEM
Wilikina Drive Sta. 15+89± (o/s 11' LT)



DETAIL-INERTIAL BARRIER SYSTEM
McCornack Rd. Sta. 23+15± (o/s 9' LT)



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ME OR UNDER MY SUPERVISION
Peter L. Now
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STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

TRAFFIC CONTROL PLANS

**WILIKINA DR INTERSECTION IMPROVEMENTS
AT McNAIR GATE**

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SHEET No. 2 OF 2 SHEETS