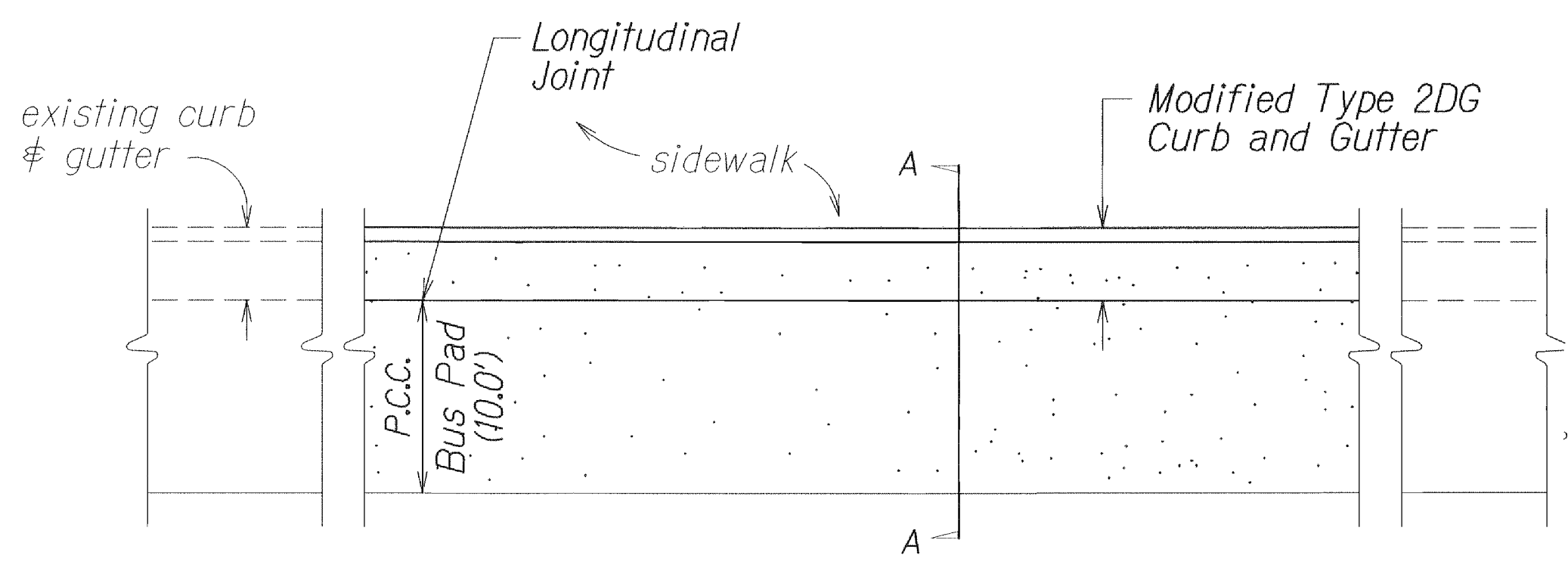
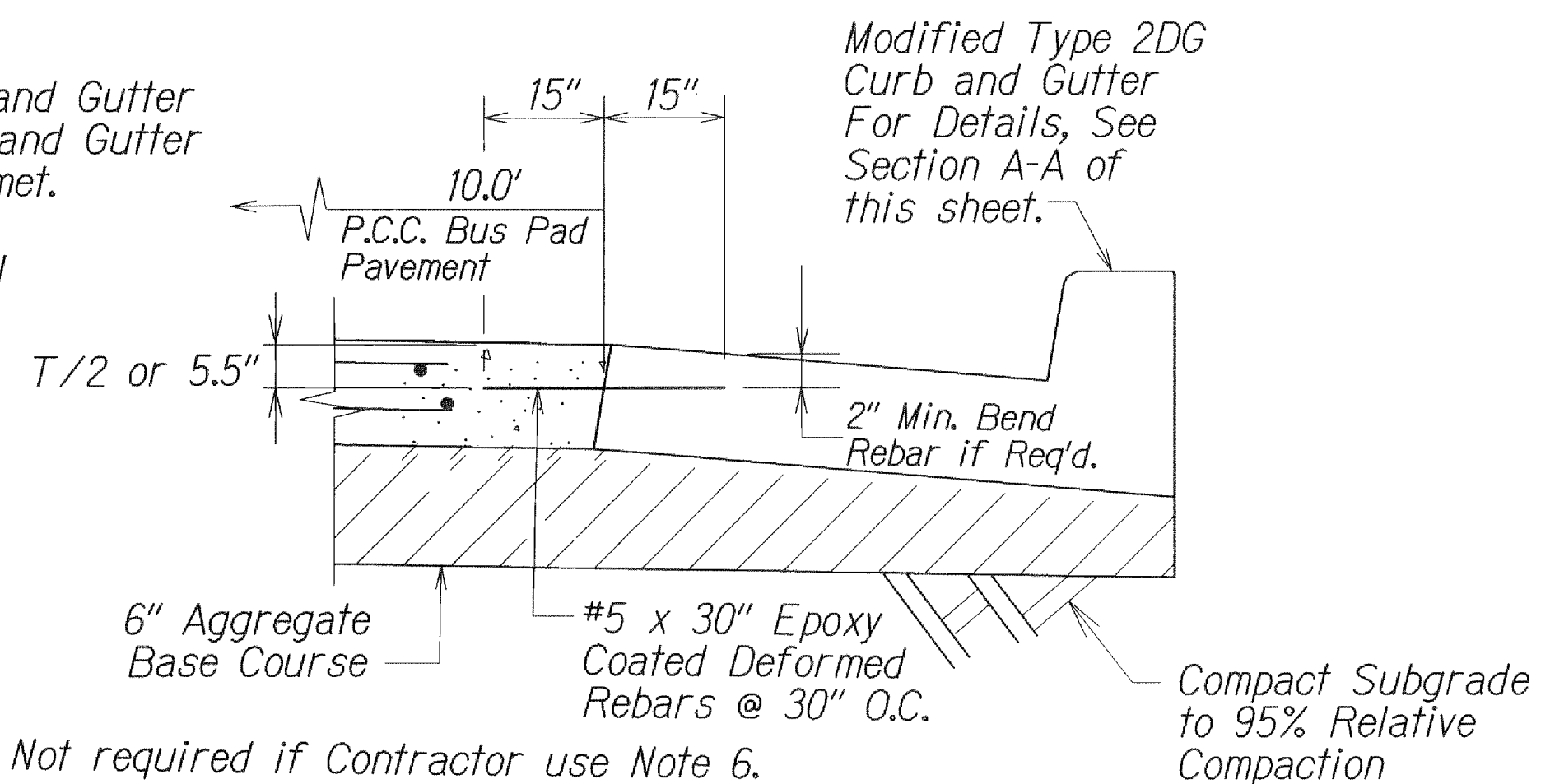


FED. ROAD DIST. NO.	STA. TE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	STP-098-1(011)	2012	16	64

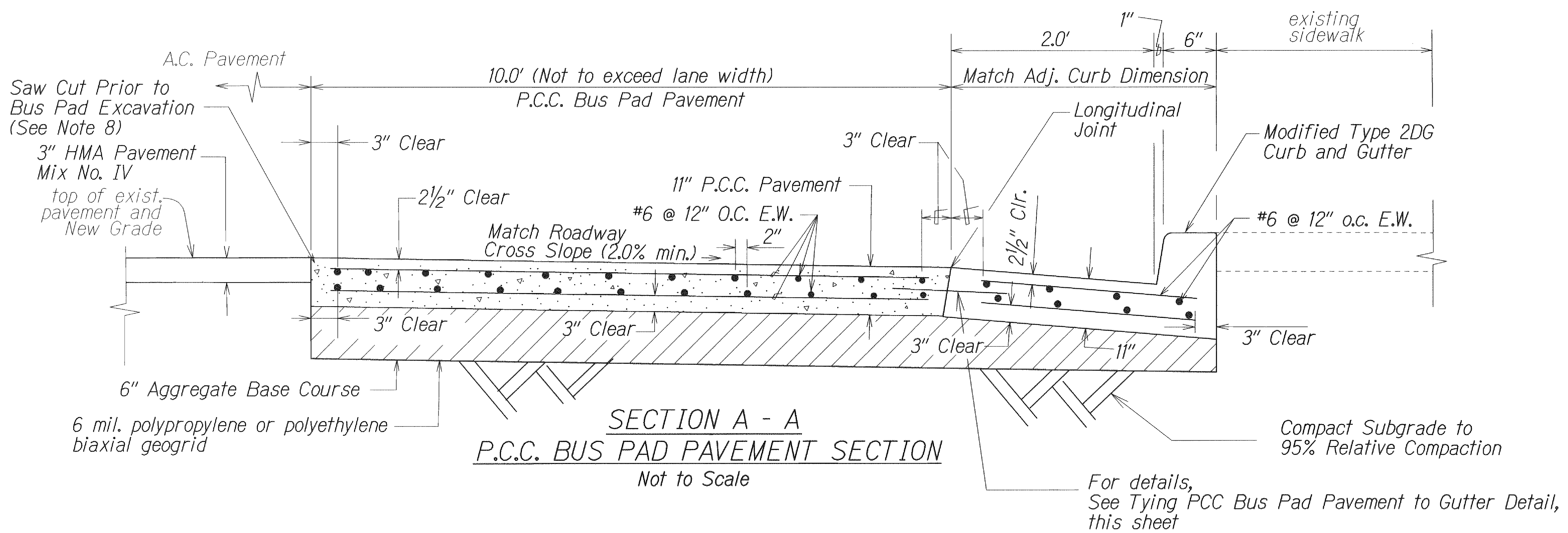


**TYPICAL JOINT LOCATION AND LAYOUT
PLAN FOR P.C.C. BUS PAD PAVEMENT**
Not to Scale

- Notes:
1. Provide continuously reinforced Bus Pad without transverse construction joints. Transverse construction joints with load transfer devices or dowels are required for P.C.C. Bus Pad Pavement greater than 70 feet in length. Provide 3" clearance between the rebar and the edge of pavement.
 2. Provide a constant grade match roadway cross slope (2% min.) on the New P.C.C. Bus Pad Pavement and match existing A.C. Pavement to the New Bus Pad within the transition area.
 3. Epoxy coated reinforcement shall conform to AASHTO M 284, (ASTM 775).
 4. Concrete shall attain a minimum 14-day flexural strength, f_r -650 psi.
 5. Roadway excavation pavement required for the installation of P.C.C. Bus Pad Pavement and disposal of excavated materials shall be considered incidental to P.C.C. Bus Pad and will not be paid for separately.
 6. P.C.C. Bus Pad can be poured monolithically together with the Curb and Gutter and the rebar pattern can continue from the bus pad into the Curb and Gutter (Same rebar arrangement) provided the plan details and slopes are met.
 7. Removal and Demolishing of existing Type 2DG Curb and Gutter shall be considered incidental to New Modified Type 2DG Curb and Gutter.
 8. Forms shall be used for the concrete bus pad, and the A.C. should not be used as a form. Following the setting of the concrete, the forms should be removed and filled with properly compacted HMA Pavement Mix No. IV.



**TYING P.C.C. BUS PAD PAVEMENT
TO GUTTER DETAIL**
Not to Scale



**SECTION A - A
P.C.C. BUS PAD PAVEMENT SECTION**
Not to Scale

DATE	_____
SURVEY PLOTTED BY	_____
DESIGNED BY	_____
QUANTITIES BY	_____
CHECKED BY	_____
ORIGINAL PLAN	_____
NOTE BOOK	_____
NO.	_____

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

P.C.C. BUS PAD DETAIL

VINEYARD BOULEVARD RESURFACING
Vicinity of Palama St. to End of H-1 On-and-off Ramp
Federal-Aid Project No. STP-098-1(011)

Not to Scale Date: November, 2012

SHEET No. 1 OF 1 SHEETS