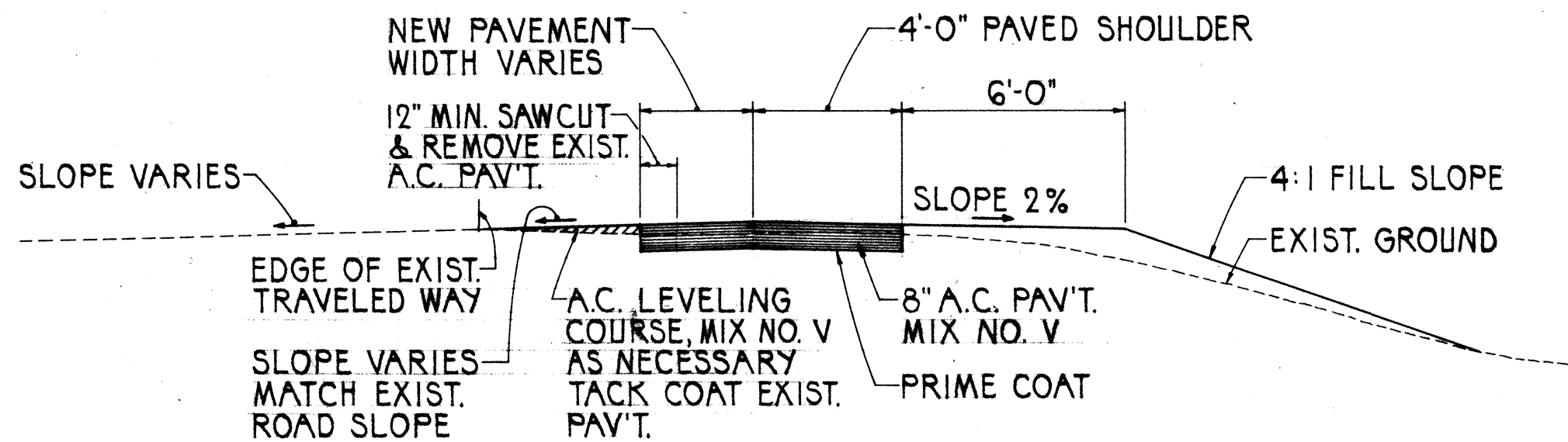
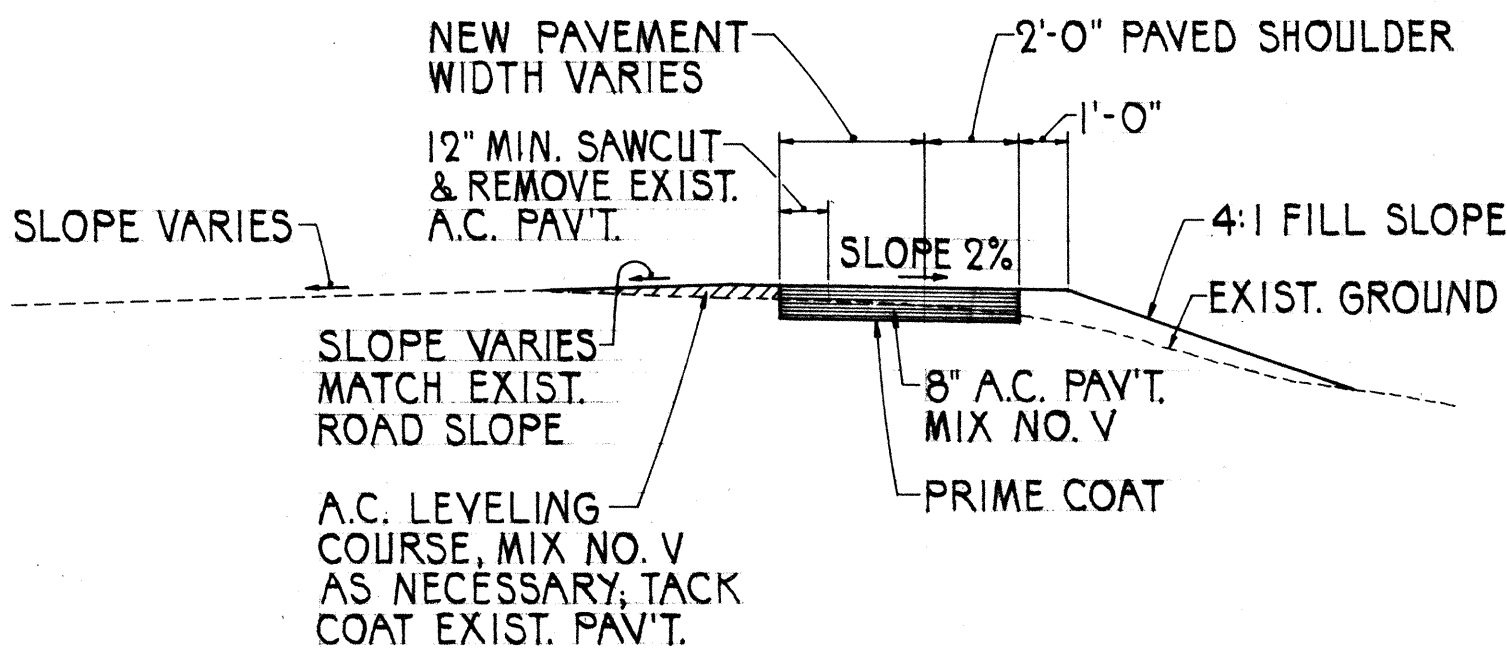


FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	93A-03-86	1986	2	20



**TYPICAL PAVEMENT SECTION**

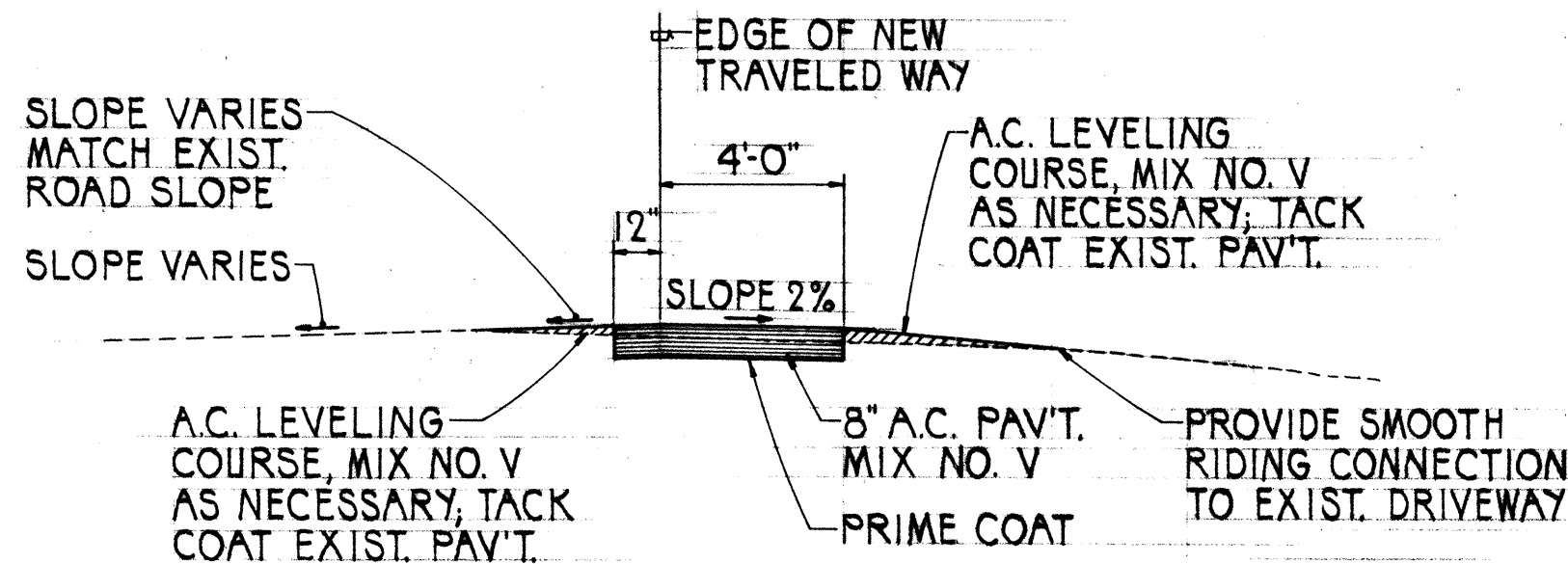
SCALE: 1/4" = 1'-0"



**TYPICAL PAVEMENT SECTION - BUS TURNOUT**

STA. 175+40 TO STA. 176+57

SCALE: 1/4" = 1'-0"



**TYPICAL PAVEMENT SECTION  
DRIVEWAY AT STA. 173+45**

SCALE: 1/4" = 1'-0"

**LEGEND**

- 12" RYG STANDARD TRAFFIC SIGNAL HEAD
- 12" RY↑ TRAFFIC SIGNAL HEAD
- STANDARD TRAFFIC AND PEDESTRIAN SIGNAL HEADS MOUNTED ON TYPE I SIGNAL STANDARD, HEIGHT = 10'
- TRAFFIC SIGNAL HEADS MOUNTED ON TYPE II SIGNAL STANDARD, ARM SPREAD SHOWN IS 30' AND DISTANCE BETWEEN SIGNAL HEADS IS 12'
- ☒ TYPE "A" PULLBOX
- TYPE "B" PULLBOX
- M TYPE "B" PULLBOX WITH MODIFIED COVER
- LOOP DETECTOR, SERIES-PARALLEL CONNECTED
- LOOP DETECTOR, SERIES CONNECTED
- LP LIGHT POLE OR STREET LIGHT STANDARD
- PP POWER POLE
- TP TELEPHONE POLE
- WV WATER VALVE
- WMH WATER MANHOLE
- WM WATER METER
- SMH SEWER MANHOLE
- FH FIRE HYDRANT
- \*\*\*\*\* EXISTING STRIPING AND MARKERS TO BE REMOVED
- TS NEW TRAFFIC SIGNAL STANDARD
- ||||| EXISTING CROSSWALK MARKINGS
- ↗ NEW PAVEMENT ARROW
- ||||| NEW A.C. PAVEMENT
- EXISTING MARKINGS
- NEW STRIPING
- W12 EXISTING UTILITY LINES AND SIZES AS INDICATED

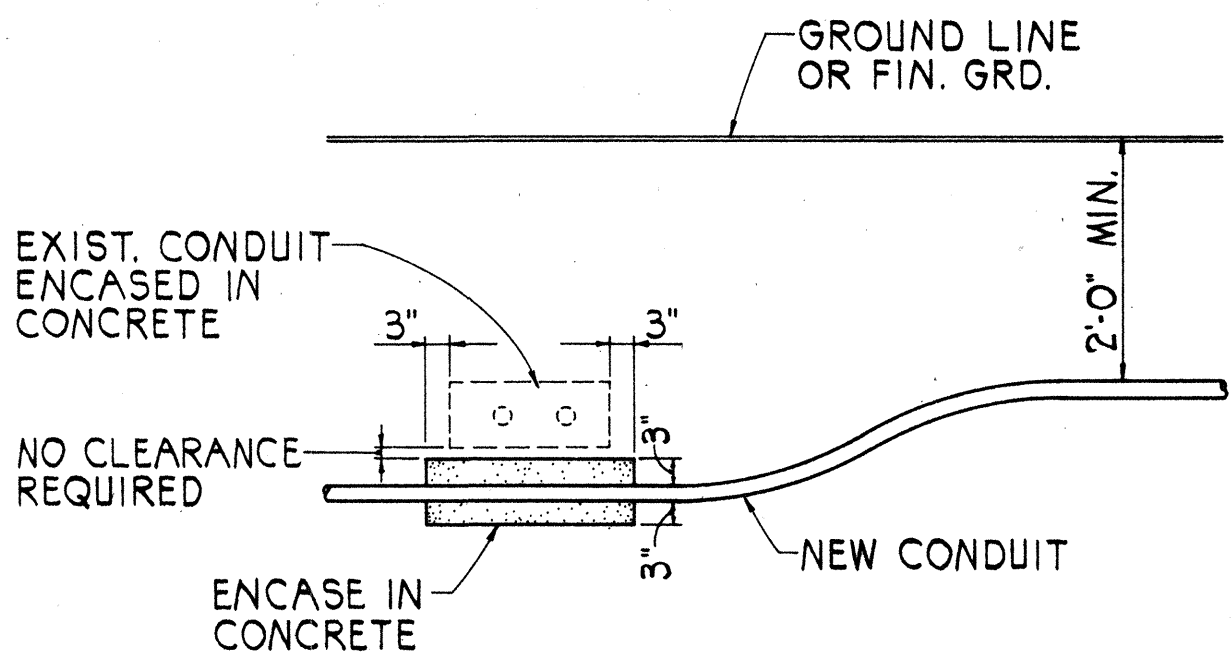
W = WATER  
S = SEWER  
D = DRAIN  
SC = SIGNAL CORPS

**CONSTRUCTION NOTES**

- LOCATIONS OF EXISTING UNDERGROUND STRUCTURES AND UTILITIES SUCH AS PIPELINES, CONDUITS, CABLES, ETC., SHOWN ON PLANS ARE APPROXIMATE ONLY. IT IS NOT THE INTENT OF THESE PLANS TO SHOW THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES AND STRUCTURES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATIONS OF ALL EXISTING UTILITIES WITH THE RESPECTIVE OWNERS. EXISTING UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN COST.
- THE LOCATIONS OF THE TRAFFIC SIGNAL STANDARDS, TRAFFIC SIGNAL STANDARDS WITH MAST-ARM, PEDESTRIAN PUSH BUTTONS, TRAFFIC CONTROLLER CABINET, PULLBOXES, CONDUITS AND LOOP DETECTORS SHALL BE STAKED OUT IN THE FIELD BY THE CONTRACTOR AND APPROVAL OF THE LOCATIONS SHALL BE OBTAINED FROM THE ENGINEER PRIOR TO CONSTRUCTION AND INSTALLATION.
- ALL TRAFFIC SIGNAL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", FEDERAL HIGHWAY ADMINISTRATION (1984) AND AMENDMENTS.
- LOCATIONS OF TRAFFIC MARKINGS AND MARKERS (LANE LINES, STOP LINES, CROSSWALK, ETC.) SHOWN ON THE PLANS SHALL BE VERIFIED WITH THE ENGINEER PRIOR TO THE INSTALLATION OF THE TRAFFIC SIGNAL SYSTEM.
- REMOVAL OF PAVEMENT MARKINGS AND STRIPINGS SHALL BE DONE BY THE CONTRACTOR.
- MAINTENANCE OF TRAFFIC THROUGH THE CONSTRUCTION AREA SHALL BE IN ACCORDANCE WITH PART VI OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", FEDERAL HIGHWAY ADMINISTRATION (1984) AND AS SPECIFIED IN THE SPECIAL PROVISIONS. THE CONTRACTOR SHALL FURNISH AND MAINTAIN ADEQUATE BARRICADES, BLINKERS, CONSTRUCTION SIGNS, ETC., FOR THE SAFETY OF THE MOTORING PUBLIC.
- DEPARTMENT OF TRANSPORTATION SERVICES, CITY AND COUNTY OF HONOLULU, WILL ASSIST THE ENGINEER IN CONSTRUCTION INSPECTION FOR THE TRAFFIC SIGNAL SYSTEM.
- REMOVAL OF EXISTING SIGNS SHALL ALSO INCLUDE THE REMOVAL OF POSTS AND FOUNDATIONS UNLESS OTHERWISE NOTED. ALL SUCH MATERIALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR. COSTS SHALL BE INCIDENTAL TO OTHER ITEMS OF WORK.

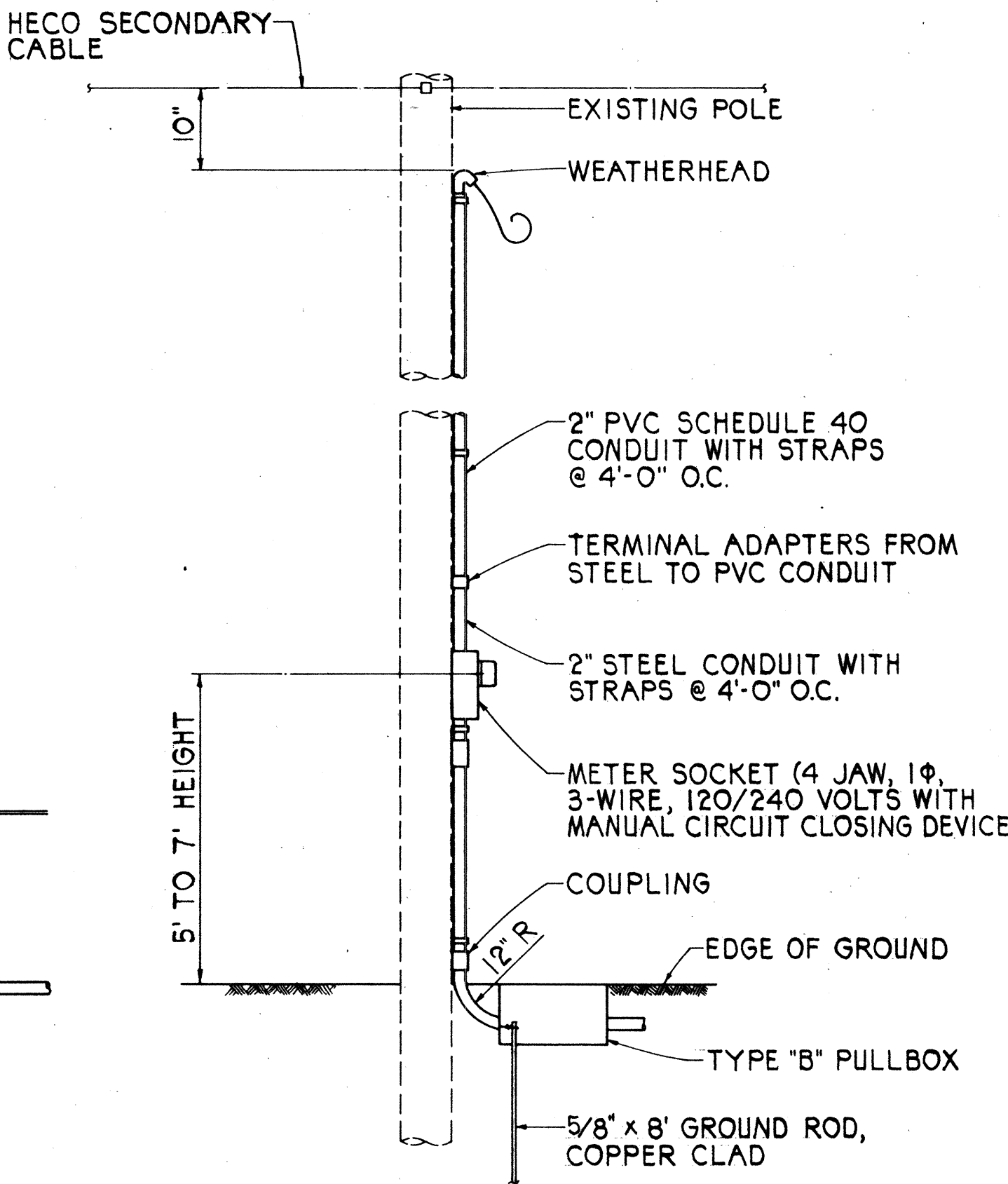
**TRAFFIC SIGNAL NOTES**

- ALL TRAFFIC SIGNAL CONTROLLER EQUIPMENT SHALL BE COMPLETELY WIRED IN THE CABINET AND SHALL CONTROL THE TRAFFIC SIGNALS AS CALLED FOR IN THE PLANS.
- SIGNAL INDICATIONS DURING CLEARANCE INTERVAL:
  - IF A SIGNAL IS G OR <G> AND WILL REMAIN G OR <G> DURING THE NEXT PHASE, IT SHALL BE G OR <G> DURING THE CLEARANCE INTERVAL.
  - IF A SIGNAL IS G OR <G> AND WILL BECOME R OR EXTINGUISHED DURING THE NEXT PHASE, IT SHALL BE Y OR <Y> DURING THE CLEARANCE INTERVAL.
  - IF A SIGNAL IS R AND WILL REMAIN R OR BECOMES G DURING THE NEXT PHASE, IT SHALL REMAIN R DURING THE CLEARANCE INTERVAL.
- A SOLID #8 BARE COPPER WIRE SHALL BE PULLED WITH THE TRAFFIC CONTROL CABLE FOR EQUIPMENT GROUND. COST SHALL BE INCIDENTAL TO THE INSTALLATION OF THE CONTROL CABLE.
- CONTRACTOR SHALL INSTALL METER SOCKET AND BREAKER ON POWER POLE AS SHOWN ON PLANS IN ACCORDANCE WITH HECO REQUIREMENTS. METER SHALL BE MOUNTED BETWEEN 5' AND 7' ABOVE GROUND. METER SOCKET SHALL BE 4-PRONG, COMPLETE WITH A MANUAL CIRCUIT CLOSING DEVICE.



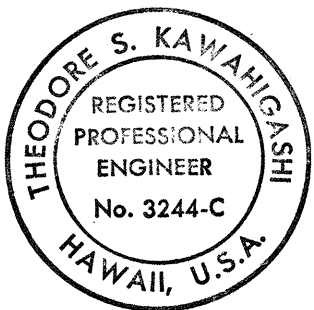
**CONDUIT BY-PASS DETAIL**

NOT TO SCALE



**SERVICE POLE DETAIL**

NOT TO SCALE



THIS WORK WAS PREPARED BY ME  
OR UNDER MY SUPERVISION

*Theodore S. Kawahigashi*  
Signature

STATE OF HAWAII  
DEPARTMENT OF TRANSPORTATION  
HIGHWAYS DIVISION

**TYPICAL PAVEMENT  
SECTION AND NOTES**

FARRINGTON HIGHWAY  
TRAFFIC SIGNALS AT  
HOOKELE STREET  
PROJECT NO. 93A-03-86

SCALE: AS NOTED DATE: MAY 16, 1986

SHEET No. 1 OF 1 SHEETS