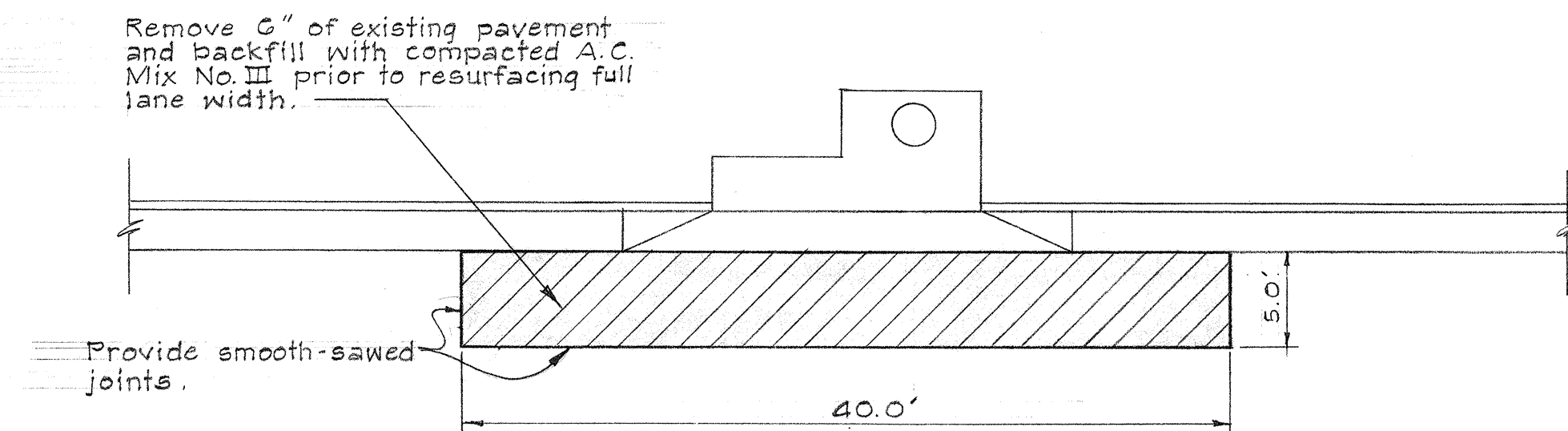


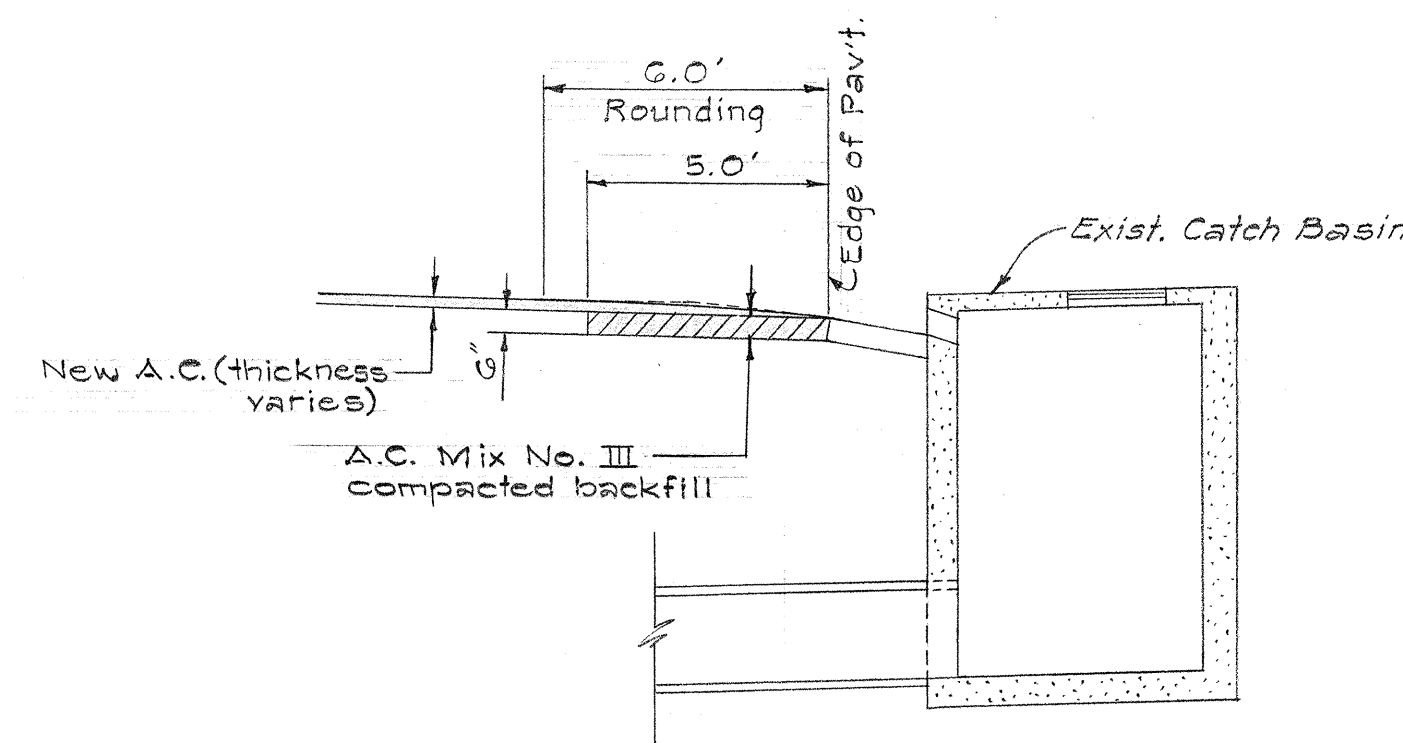
GENERAL NOTES

LEGEND

- Denotes resurfacing over existing asphalt concrete pavement.
- Denotes resurfacing over existing P.C.C. pavement.
- Denotes area for reconstruction of pavement and/or base course.



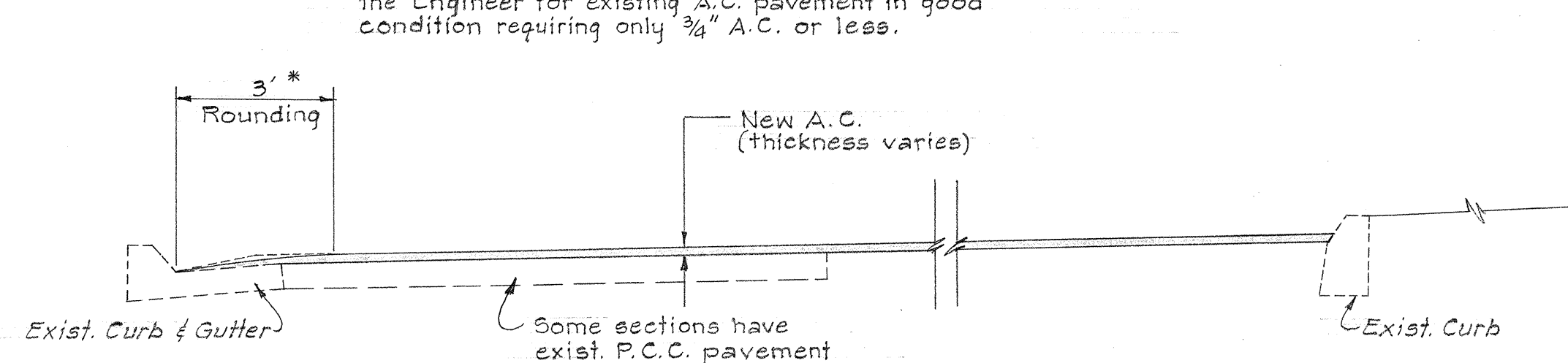
PLAN
Scale: $\frac{1}{8}'' = 1'-0''$



SECTION
Scale: $\frac{1}{4}'' = 1'-0''$

DETAILS FOR RECONSTRUCTION
AT CATCH BASINS No. 10 THROUGH 19
Scale: As Noted

* A.C. over gutter may be reduced or eliminated by the Engineer for existing A.C. pavement in good condition requiring only $\frac{3}{4}''$ A.C. or less.



TYPICAL HALF SECTION DETAIL FOR
RESURFACING AT CURB AND GUTTERS
Scale: $\frac{3}{8}'' = 1'-0''$

- The existing pavement shall be broomed and tack coated and then the prepared surface shall be resurfaced with asphalt concrete to the thicknesses specified in the Resurfacing Schedule.
- In the preparation for resurfacing, additional work will be required at certain locations as follows:
 - Cracks, depressions and holes encountered in the existing pavement shall be cleaned of loose material and filled and compacted with Mix No. V or Mix No. III asphalt concrete.
 - Reconstruction of existing pavement prior to resurfacing will be required at certain catch basins, manholes, cracked P.C.C. pavement and at approaches to existing railroad tracks, all as shown on the plans and in accordance with the Special Provisions and/or Standard Specification. Removal of existing pavement required for reconstruction of existing pavements at certain locations will be paid for under "Roadway Excavation."
- Smooth riding connections shall be provided at the beginning and end of project and also at various side street connections. Smooth transitions shall also be provided between varying resurfacing thickness shown on the Resurfacing Schedule.
- All medial openings and acceleration and deceleration lanes shall be resurfaced.
- All existing centerline, baseline or reference monuments shall be referenced before resurfacing and exposed after resurfacing. Monuments with frame and cover shall be raised. This work is considered to be incidental to Asphalt Concrete Pavement and no separate payment will be made thereof.
- Existing railroad tracks in the vicinity of Baseline Station GG+60± shall not be disturbed or paved over since they are currently being used as a railroad crossing by Oahu Railway & Terminal Warehousing Co. Ltd.
- Before paving, the Contractor shall reference all existing manholes in a manner satisfactory to the Engineer. In connection with this work, the Contractor shall notify all applicable utility agencies and/or companies including the U. S. Army (Signal Corps) prior to beginning paving operations. All of this work is considered to be incidental to Asphalt Concrete Pavement and no separate payment will be made thereof.
- Resurfacing during peak traffic hours, 6:30 a.m. to 8:30 a.m. and 3:30 p.m. to 5:30 p.m. (Monday through Friday), will not be permitted and all six lanes of Nimitz Highway shall be kept open to traffic. At all other times, resurfacing shall be limited to one lane each in the inbound and outbound lanes.
- Approximately 1,020 lineal feet of existing A.C. rolls at various locations shall be removed and disposed of outside the highway right-of-way by the Contractor prior to resurfacing, where directed by the Engineer. Cost of removing, hauling and disposing of the A.C. rolls shall be considered as incidental to the various contract items and no separate payment will be made thereof. (Note 2 added to GENERAL NOTES per Addendum No. 1. Posted March 11, 1965.)

RESURFACING SCHEDULE				
LOCATION		A.C. THICKNESS IN INCHES		
Highway Lanes	Station to Station	Planned	POSSIBLE VARIATION After * Bid Opening	As-Built
I.B. & O.B.	Kalihi Stream Bridge	$\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 1"	
I.B. & O.B.	± 195+76.22 to 205+10	$\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 1"	
I.B. & O.B.	± 205+10 to 207+20.79	$1\frac{1}{4}''$ Mix No. V	$\frac{3}{4}''$ to 2 $\frac{1}{2}''$	
I.B. & O.B.	± 13+86.43 to 33+00	$1\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 2 $\frac{1}{2}''$	
I.B. & O.B.	± 33+00 to 54+32.79	$2\frac{1}{4}''$ ^{$1\frac{3}{4}''$ Mix No. V} _{$1\frac{1}{2}''$ Mix No. III}	$\frac{3}{4}''$ to 3"	
I.B. & O.B.	± 54+32.79 to 62+25 ±	$1\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 2 $\frac{1}{2}''$	
I.B. & O.B.	Kapaemahu Canal Bridge	$\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 1"	
I.B.	± 63+69 ± to 84+65.16	$1\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 2 $\frac{1}{2}''$	
O.B.	± 63+69 ± to 84+65.16	$\frac{3}{4}''$ Mix No. V	$\frac{3}{4}''$ to 1"	

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

GENERAL NOTES
AND
MISCELLANEOUS DETAILS

NIMITZ HIGHWAY
Project No. A-92-01-65

Scale: As Noted Date: Feb. 1965

SHEET No. OF SHEETS