

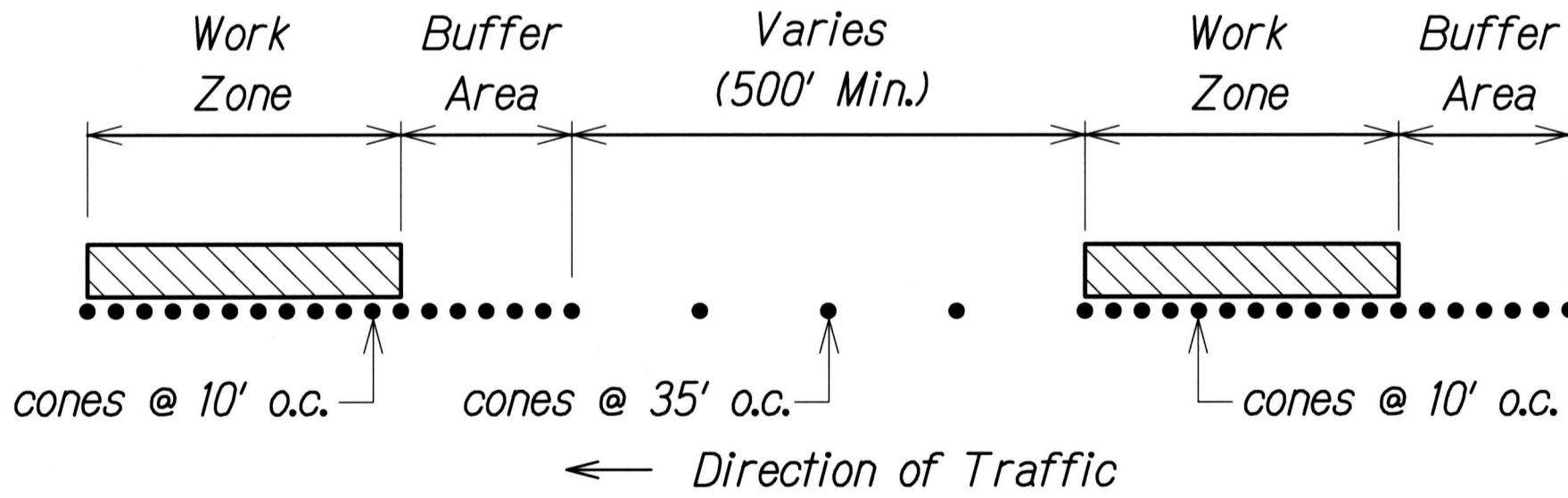
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	83G-01-17M	2017	35	48

**GENERAL NOTES FOR TRAFFIC CONTROL PLAN
(FOR STATE RIGHT-OF-WAY)**

- Only Traffic Control Plans for major construction activities are shown. The Contractor shall develop his own Traffic Control Plans in accordance with Section 645 of the Special Provisions or the current US Federal Highways Administration "Manual on Uniform Control Devices, Part 6-Temporary Traffic Control" (MUTCD 2009) for activities to complete work not covered by the Traffic Control Plans for the sites listed in the "Traffic Control Schedule" table. The Contractor shall follow the more stringent guidelines. The Contractor shall submit the Traffic Control Plans to the Engineer for approval. Payment for development and implementation of the Traffic Control Plans shall be considered incidental to the various contract items.
- All lane closures and traffic pattern changes (detours) not shown on the plan shall be submitted to the Engineer for approval in accordance with Specifications Section 645 - Work Zone Traffic Control. For restrictions on lane closures, detours, construction work during peak hours, and other requirements regarding maintaining vehicular and pedestrian traffic, see Section 107.06 - Contractor Duty Regarding Public Convenience, and Section 645 - Work Zone Traffic Control. Construction materials and equipment should be transferred to and from the project site during off-peak traffic hours (8:30 a.m. to 3:30 p.m. Monday to Friday) to minimize any possible disruption to traffic on the local streets.
- Sites requiring lane closures are as indicated in the plans. Additional lane closures are prohibited unless otherwise approved by the Engineer. Fees for unauthorized lane closures shall be borne by the Contractor, in accordance with Section 108-Prosecution and Progress of the Special Provisions.
- The Contractor shall make minor adjustments at intersections, driveways, bridges, structures, etc. to fit field conditions.
- Cones or delineators shall be extended to a point where they are visible to approaching traffic.
- Traffic control devices shall be installed such that the sign or device farthest from the work area shall be placed first. The others shall then be placed progressively toward the work area. Likewise, removal of traffic control should begin with the device closest to the work area and progressively away from the work area.
- Regulatory and warning signs within the construction zone that are in conflict with the traffic plans shall be removed or covered.
- Flaggers and/or police officers shall be in sight of each other or in direct communications at all times.
- Sign spacings (L), taper lengths (T), and spacings of cones or delineators shall be as shown in Table 1 of Section 645 in the Specifications, unless otherwise noted on HDO's Traffic Control Plans.
- All traffic lanes shall be a minimum of 11 feet wide.
- All signs shall be promptly removed or covered whenever the message is not applicable or not in use.
- The backs of all signs for traffic control shall be appropriately covered to preclude the display of inapplicable sign messages (i.e., when signs have messages on both faces).
- At the end of each day's work or as soon as the work is completed, the Contractor shall remove all traffic control devices no longer needed to permit free and safe passage of public traffic. Removal shall be in the reverse order of installation.

**GENERAL NOTES FOR TRAFFIC CONTROL PLAN
(FOR STATE RIGHT-OF-WAY) (CONT.)**

- Replace permanent pavement markings and traffic signs upon completion of each phase of work. Temporary pavement markings and traffic signs shall be used in the interim.
- Existing conflicting pavement markings shall be removed and temporary pavement markings shall be installed before traffic patterns are changed. After completion of the work, temporary pavement markings shall be removed. Payment for removal of existing pavement markings; and furnishing, installing, and removal of temporary pavement markings, whether or not shown on the Traffic Control Plans shall be considered incidental to the various contract items.
- The locations of pavement markings, signs, and delineators used in the Traffic Control shall be as shown on the plans, Contractor's approved Traffic Control Plans, and/or as determined in the field by the Engineer.
- Damage to signs, temporary pavement markers, and delineators caused by the public or Contractor's negligence shall be repaired or replaced by the Contractor at the Contractor's expense.
- Signs for night work shall be retroreflective and shall be mounted with a Type B high intensity flasher.
- Work zone limits shown for each traffic control phase encompass all work items to be completed in that particular phase. The length of the work zone may be reduced to accommodate the Contractor's actual work zone for that time period, provided it has been accepted by the Engineer, and all tangents, tapers, and buffer lengths are maintained.
- The Contractor is responsible for providing all sign supports and/or posts for construction warning signs.
- For work involving installation of catch basin devices, lane closures will only be allowed from 8:30 am to 1:00 pm on Monday, Tuesday, Thursday, and Friday, and from 8:30 am to 12:30 pm on Wednesday.
- All driveways shall be kept open unless the owners of the property using these driveways are otherwise provided for satisfactorily.
- For intermittent work zones within a particular traffic control phase, only cones along the actual work zone need to be placed at 10' o.c. The intermittent work zone shall include a 220' buffer area along the leading edge of traffic flow. All other traffic control cones can be placed at 35' o.c. See typical detail below.



**TYPICAL CONE SPACING FOR INTERMITTENT
WORK ZONE**
Not to Scale

LEGEND

- Sign
- Existing Sign
- Direction of Traffic
- Cones with Retroreflective bands (All cones in work area shall be 28" high).
- Work Area
- Water-filled Barricade
- Police Officer

DATE	_____
SURVEY PLOTTED BY	_____
DRAWN BY	_____
TRACED BY	_____
QUANTITIES BY	_____
CHECKED BY	_____
ORIGINAL PLAN	_____
NOTE BOOK	_____
No.	_____

TC-01 TCR NOTES/83G 4/17/2017 12:05 PM

4/30/18
EXP. DATE

This work was prepared by me or under my supervision.

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

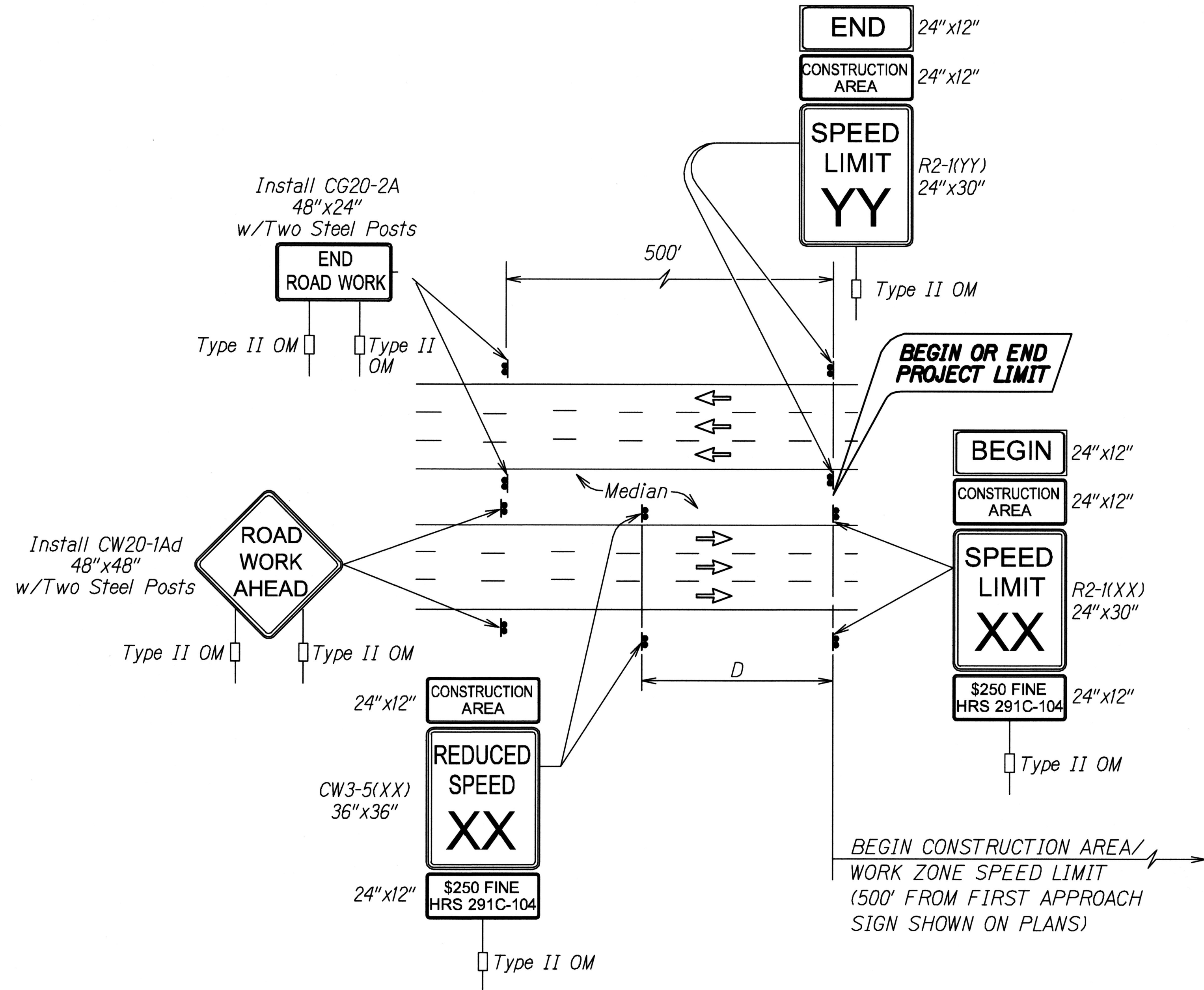
**TRAFFIC CONTROL
NOTES**

KAWA WATERSHED STORM WATER
BEST MANAGEMENT PRACTICES ON OAHU, PHASE 1

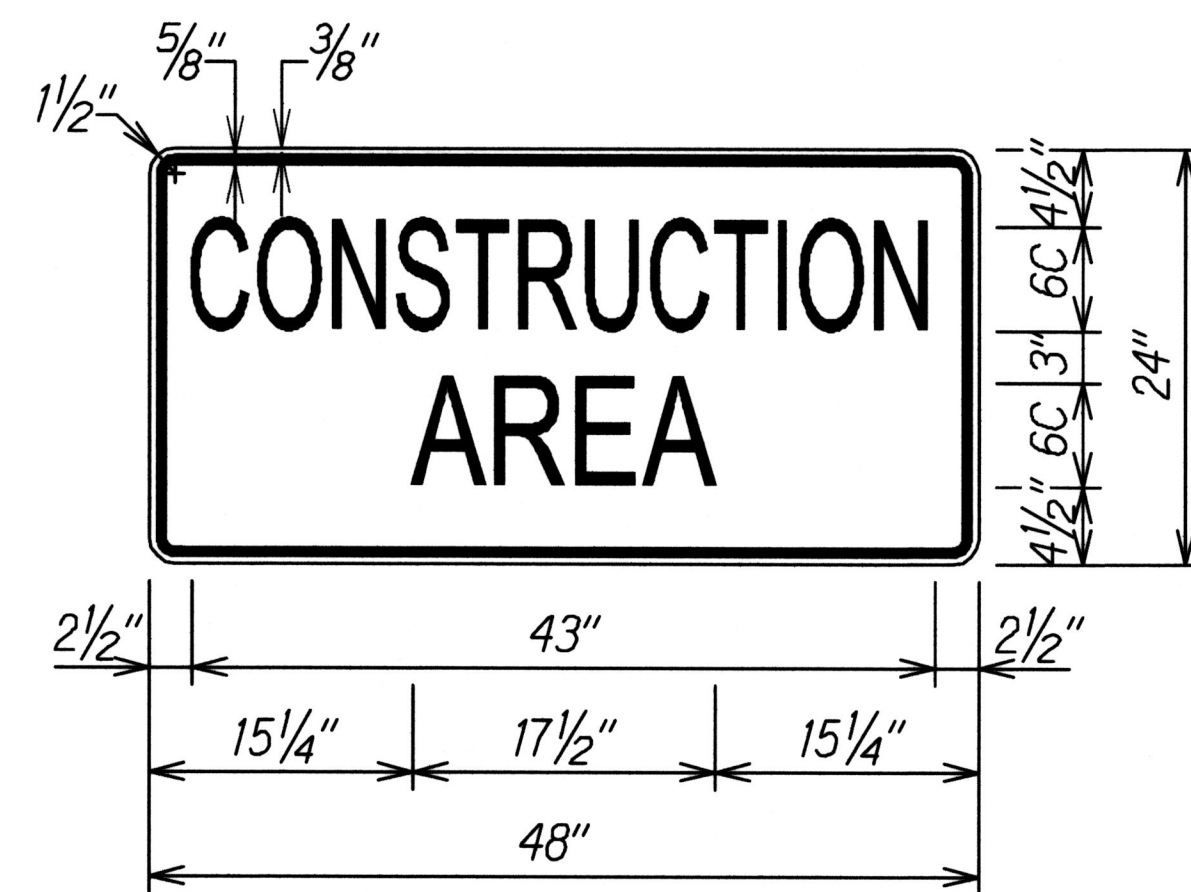
Project No. 83G-01-17M

Scale: None Date: April 2017

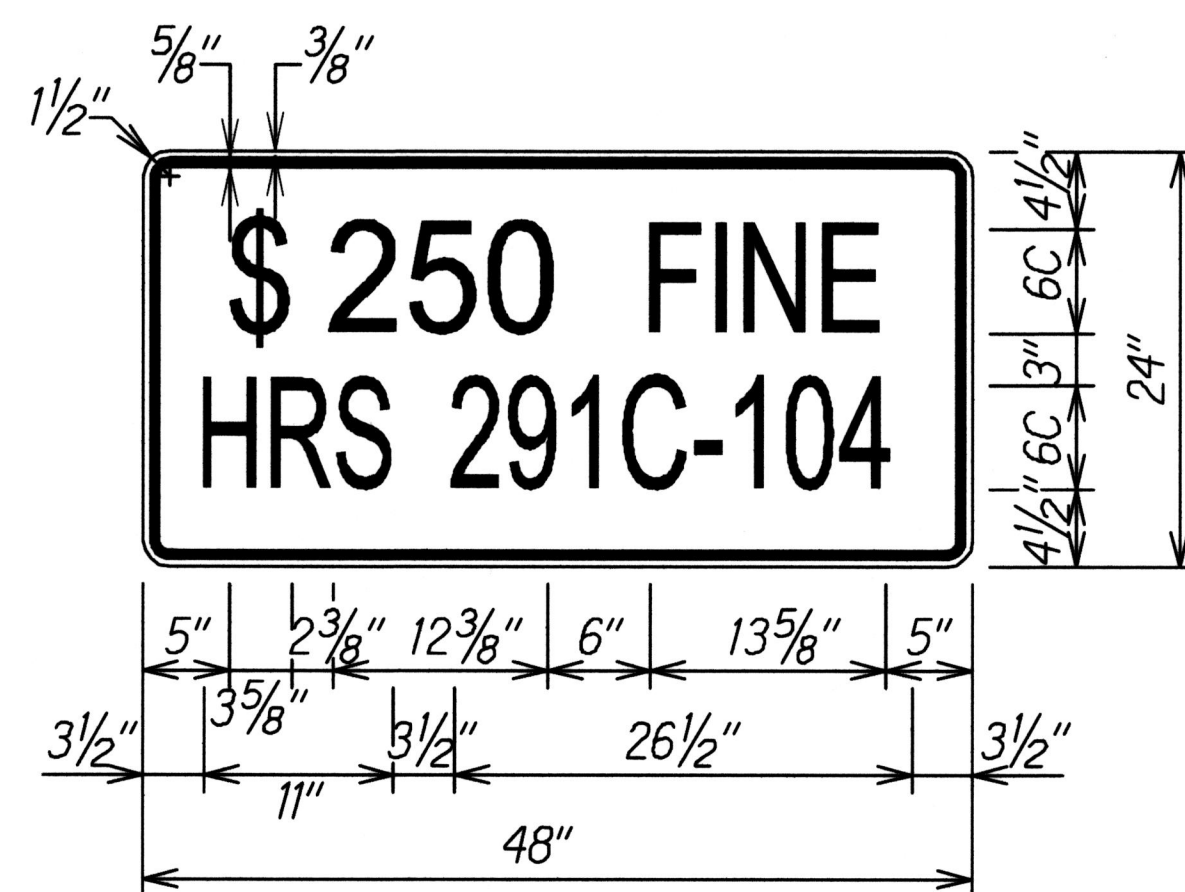
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	83G-01-17M	2017	36	48



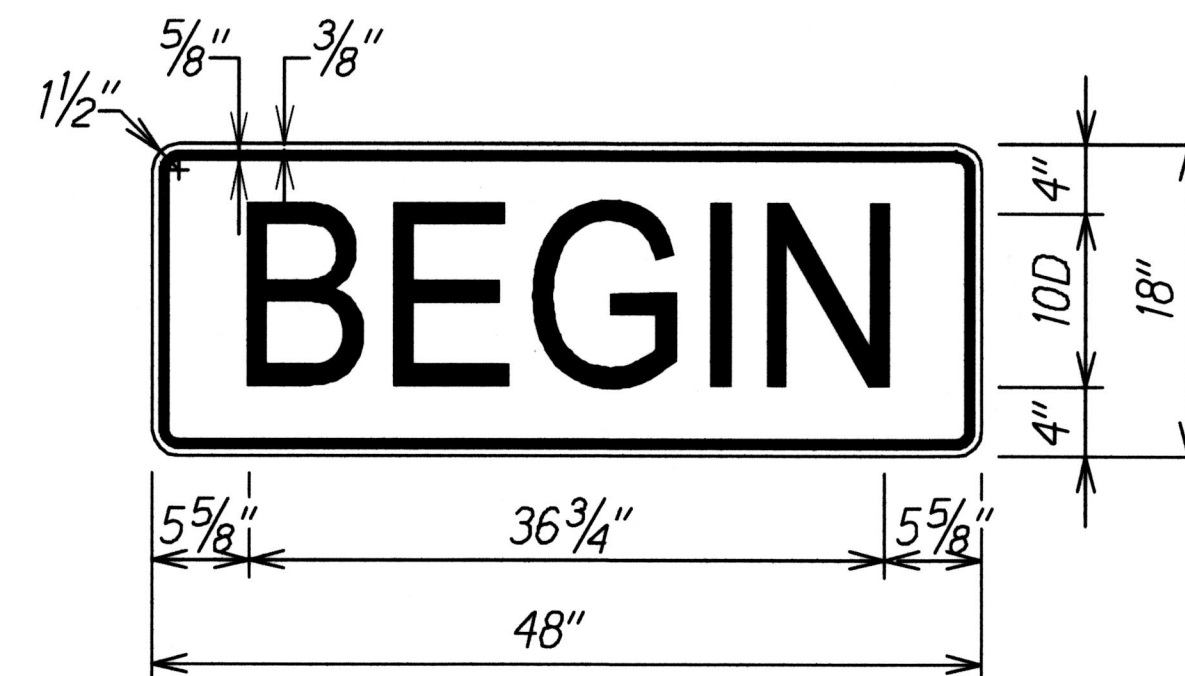
**TYPICAL DETAIL FOR CONSTRUCTION SIGNS
ON MULTILANE DIVIDED LOW SPEED HIGHWAY**



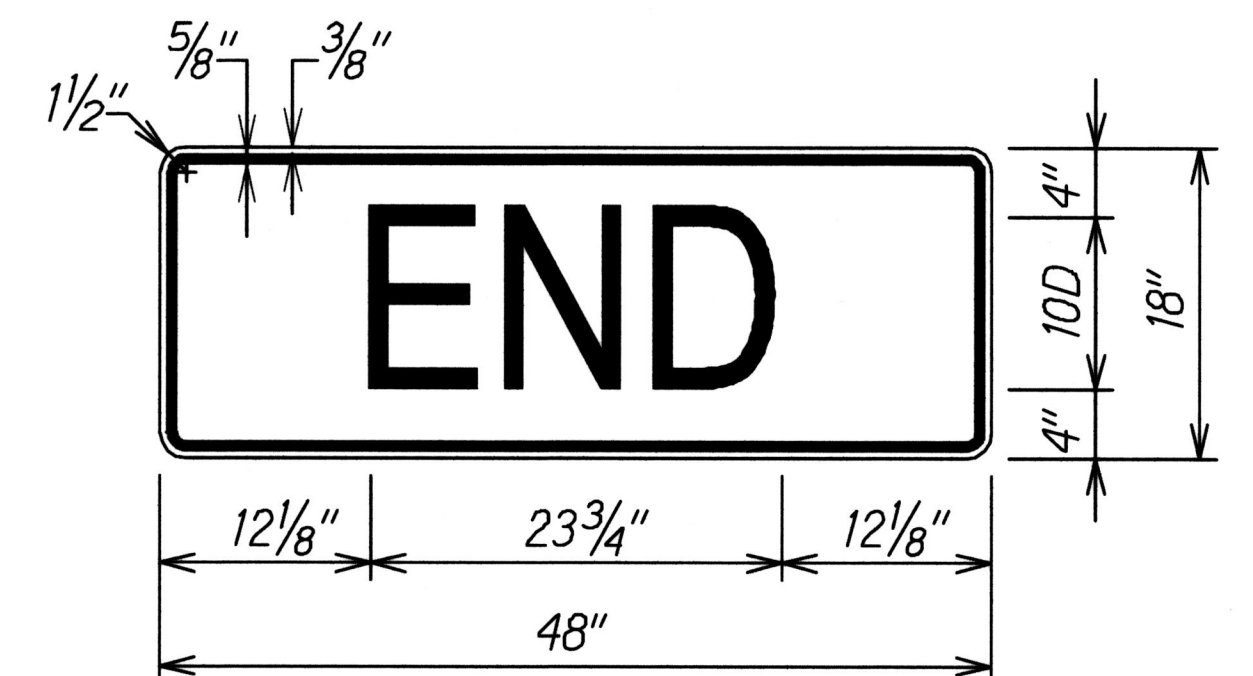
LEGEND: BLACK
BACKGROUND: ORANGE



LEGEND: BLACK
BACKGROUND: WHITE



LEGEND: BLACK
BACKGROUND: ORANGE



LEGEND: BLACK
BACKGROUND: ORANGE

Work Zone Note:

1. This Work Zone Sign Plan is intended for use on long-term stationary work zones/construction phases (3 days or more). All work zones or construction phases less than 3 days duration will use Traffic Control Plans shown in Section 645 of the Standard Specifications.
2. All existing regulatory speed limit signs with posts within the work zone/project limits shall be removed and replaced with work zone speed limit sign assemblies (R2-1(25) and R2-5b(25) with "CONSTRUCTION AREA" and "\$250 FINE HRS 291C-104" Supplemental Signs).
3. Construction sign assemblies shall be installed on both the approaching and trailing ends of each work zone as shown on this plan.
4. Each construction warning sign shall have a minimum of two (2) Type II OM. Each work zone speed limit assembly shall have a minimum of one (1) Type II OM. Installation of each Type II OM shall be considered incidental to the various contract items.
5. Upon the completion of all physical work or as directed by the Engineer, all construction signs and work zone speed limit assemblies shall be removed. All speed limit signs and posts that were existing at the start of the project within the work zone/project limits shall be restored back to their original locations and configurations.
6. Placement of construction signs shall not obstruct the path of pedestrians and bicyclists.
7. The removal and restoration of existing regulatory speed limit signs with new posts along with the installation, maintenance and removal of work zone speed limit sign assemblies shall be considered incidental to the various contract items.

TABLE 1 GUIDELINE FOR ADVANCE PLACEMENT OF SPEED REDUCTION SIGN R2-5b(XX)-A, CONSTRUCTION AREA SPEED LIMIT		
YY-EXISTING POSTED SPEED LIMIT (MPH)	XX-NEW CONSTRUCTION AREA SPEED LIMIT (MPH)	D-REDUCED SPEED LIMIT SIGN SPACING TO FIRST CONSTRUCTION AREA SPEED LIMIT SIGN (FEET)
<25	EXIST. SPEED LIMIT TO REMAIN	
30	25	100
35	25	175
40	30	175

SURVEY PLOTTED BY:	DATE:
DRAWN BY:	DATE:
CHECKED BY:	DATE:
NOTE BOOK	QUANTITIES BY
NO.	CHECKED BY

TC-02 TC NOTES-LOW SPEEDING 2/1/2017 10:05 PM

4/30/18
EXP. DATE
This work was prepared by me or under my supervision.

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

**LOW SPEED DIVIDED HIGHWAY
WORK ZONE SIGNING PLAN, NOTES & DETAILS**

KAWA WATERSHED STORM WATER
BEST MANAGEMENT PRACTICES ON OAHU, PHASE I
Project No. 83G-01-17M
Scale: None Date: April 2017