

Attachment 9

Categorical Exclusion Approval

State of Hawaii
Department of Transportation
Highways Division

DOCUMENTATION FOR CATEGORICAL EXCLUSIONS
LISTED UNDER 23 CFR §771.117(d)

Project Title: KAIPAPAU STREAM BRIDGE REPLACEMENT (State Route 83)

1) DESCRIPTION

Attach project location map and other appropriate graphics.

Estimated Project Cost (\$mil) ROW: \$ <u>0.33</u> CON: \$ <u>11.55</u>	Project Length <u>110 ft</u> mi (<u> </u> km)	Number of Lanes Existing <u>2</u> Proposed <u>2</u>
Design Speed <u>35</u> Existing <u>35</u> Proposed	Functional Classification - 1= Principal Arterial, 2 = Minor Arterial, 3= Major Collector, 4= Minor Collector, 5= Local Road <u>2</u> Existing <u>2</u> Proposed	Proposed Typical Section <input type="checkbox"/> Rural <input checked="" type="checkbox"/> Urban
Bridge <input checked="" type="checkbox"/> Yes <input type="checkbox"/> No	Bridge Sufficiency Rating <u>35</u>	Bridge ID: 0033000830302099

Project Description:

The Kaipapa'u Stream Bridge is located on Kamehameha Highway, State Route 83, Hau'ula, Ko'olauloa, O'ahu. This project is one in a series of bridge replacements being implemented by the State Department of Transportation (SDOT-H) and Federal Highway Administration (FHWA) along the windward coast of O'ahu. Based on the current bridge replacement program of SDOT-H, the Kaipapa'u Stream Bridge facility has a National Bridge Inventory (NBI) rating of 37 based on a scale of 1-100. This NBI rating warrants rehabilitation or replacement of the bridge. Replacement and widening of the bridge will ensure that the structure meets Federal and State bridge and roadway standards. Proposed work includes construction to increase the dimensions of the bridge to approximately 110-foot long by 57-foot wide. The widened portions of the bridge will be constructed of prestressed concrete planks with cast-in-place bridge decks. The replacement bridge will also include bicycle and pedestrian facilities. Current standards for highway speed, loading, sight distances, guard railings, and other safety measures will be used in the design of the project.

2) ISSUES

Any response in a shaded box requires items 3 - 8 to be completed. Otherwise, skip to items 7 and 8.

	YES	NO
SOCIAL-ECONOMIC FACTORS		
A. General Economics - Adverse effects on the general economics of the community.		X

	YES	NO
L. Section 106, Historical & Cultural - Adverse effects to a significant cultural and/or historical resource. (<i>Cultural and historical resources are significant only if they are on or eligible for the National Register of Historic Places.</i>)	X	
M. Section 4(f) or 6(f) Properties - Acquisition of lands under the protection of Section 6(f) of the Land And Water Conservation Act of 1965. - Use of lands or other properties under the purview of Section 4(f) unless a Programmatic Section 4(f) approval has been made by the FHWA.		X X
N. Air Quality - Anticipate Carbon Monoxide levels that exceed 90% of the Federal standards of 9 ppm in 8 hours.		X
O. Noise Quality - A noise analysis is required per 23 CFR §772.5.		X
P. Hazardous Waste - Properties with hazardous waste will be acquired.		X
Q. Visual and Aesthetic - Adverse effect to view shed.		X
R. COMMENTS		

3) PURPOSE AND NEED

Purpose and need of proposed action. Include description of existing facilities, abutting facilities, and how the action links into the overall transportation system. **When appropriate**, show that this project does not restrict consideration of alternatives for other reasonably foreseeable transportation improvements.

The Kaipapa‘u Stream Bridge is located on Kamehameha Highway, State Route 83, Hau‘ula, Ko‘olauloa, O‘ahu. State Route 83 is the only highway that provides access (for residents and commerce) to communities along the northern coast of O‘ahu. This project is one in a series of bridge replacements being implemented by the State Department of Transportation (SDOT-H) and Federal Highway Administration (FHWA) along the windward coast of O‘ahu. Based on the current bridge replacement program of SDOT-H, the Kaipapa‘u Stream Bridge facility has a National Bridge Inventory (NBI) rating of 37 based on a scale of 1-100. This NBI rating warrants rehabilitation or

replacement of the bridge in order to ensure continuous un-interrupted operations of goods and services. Replacement and widening of the bridge will ensure that the structure meets Federal and State bridge and roadway standards.

4) ALTERNATIVES

Summary of the alternatives considered and if they are not proposed for adoption, why not. (Identify which, if any, of the alternatives is the preferred alternative.)

The alternatives analysis for this project included the following approaches to addressing the deficiencies of the existing Kaipapa'u Stream Bridge:

- Alternative 1: No Action - no improvements to existing bridge.
- Alternative 2: Delayed Action - postponement of improvements for an indefinite period of time.
- Alternative 3: Rehabilitation of the Existing Bridge - repair of the existing bridge in place.
- Alternative 4: Bridge Replacement and Widening - No Detour Road: phased development to maintain two traffic lanes for the duration of the project.
- Alternative 5: Bridge Rehabilitation and Widening - No Detour Road: phased development to maintain two traffic lanes for the duration of the project.
- Alternative 6: Bridge Replacement - One-Lane Detour Road (Mauka): phased development with a one-lane detour road and a single traffic lane maintained on the existing bridge.
- Alternative 7: Bridge Replacement - Two-Lane Detour Road (Mauka): single phase with a two-lane detour road on the mauka side of the bridge that would provide two traffic lanes for the duration of the project.
- Alternative 8: Bridge Replacement - Two-Lane Detour Road (Makai): single phase with a two-lane detour road on the makai side of the bridge that would provide two traffic lanes for the duration of the project.

Based on the comparison of alternatives and evaluation of the information presented in the Final EA, Sections 2.7 through 2.11, Alternative 4 is the preferred alternative for the following reasons:

- Lowest vehicle user cost of all the alternatives
- Least ROW acquisition required of all the alternatives
- Least land disturbance

5) PUBLIC INVOLVEMENT

Briefly summarize the status and results of public involvement. Include the dates and results of coordination with local units of government, if any.

Public involvement in the subject project included: formal agency and public consultation, a public informational meeting, and public hearings held by governmental agencies for specific aspects of the project.

Public and Agency Consultation

- Review of Draft EA – 30 day comments period between Nov. 8 and Dec. 10, 2006. 23 agencies and/or individual were provided with an opportunity to comment on the project. 12 comments were received.
- Presentation to Ko'olauloa Neighborhood Board, Sept. 6, 2006
- Public Informational Meeting, August 6, 2006
- Agency Consultation regarding: Section 106, NHPA; Section 4(f), DOT; Section 107, ESA; Section 9, USGS; Section 401, CWA; Section 404, CWA; and Section 10, RHA.

6) IMPACTS

Provide a description of the impacts. Also attach coordination and concurrence letters requested (See *Instructions for Categorical Exclusions*, page 4). If the coordination letters are not attached, provide information on what coordination has taken place.

Summary of Impact, Proposed Mitigation, and Consultation:

Potential Impact & Issues	Proposed Mitigation	Consultation
Right-of-way acquisition	Additional 13 feet of ROW required requiring the acquisition of a single family home. Displacement and relocation assistance to be provided in accordance with the Uniform Relocation Assistance and Real Property Acquisition Policies Act, as amended.	Land owner consulted and advised of dislocation and relocation assistance
Flood Plain Encroachment	The bridge is in flood zone AE where base flood elevation is 10-14 feet.	None
Section 404 Permit	Permit pending Corps of Engineers decision pending public review after February 1, 2010.	ACOE determination attached
U.S. Coast Guard Permit	Communications from DOT-H to USCG requesting comments, April 20, 2007	USCG 'no comment' response October 12, 2007
Historic Preservation	Final Section 106, NHPA, decision pending final review of archaeological monitoring (January 2010)	SHPD determination attached July 2006 and May 2007
Section 4f	No impacts identified	Consultation letter attached
Section 6 (LWCF)	No impacts identified	None
Coastal Zone Management Federal Consistency Review	Coordination with State Office of Planning, October 2008	Office of Planning response November 2008.
Section 401 (CWA) Certification	Application filed:	Permit pending
Storm-water run-off	Application filed: January 2010	Permit pending
Endangered Species	No impacts anticipated	NMFS May 2007, DLNR April 2007

7) DETERMINATION☒ Categorical Exclusion

It is determined, after review of this document, and coordination with other agencies, that this project was demonstrated that no significant environmental effects will result.

☐ Environmental Assessment (EA) / Environmental Impact Statement (EIS)

It is determined, after review of this document, and coordination with other agencies, that further study is required to determine if there will be significant environmental consequences. An Environmental Assessment is required.

8) SIGNATURES**Prepared By:**

2/11/10

Date

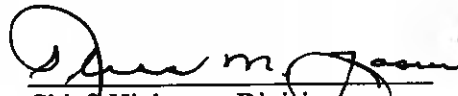


Project Manager
Hawaii Department of Transportation

Approved By:

FEB 21 2010

Date



Chief, Highways Division
Hawaii Department of Transportation

2/5/10

Date



Federal Highway Administration
Transportation Engineer