

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	63A-01-80	1980	2	16

CONSTRUCTION NOTES

- LOCATIONS OF EXISTING UNDERGROUND STRUCTURES AND UTILITIES, SUCH AS PIPELINES, CONDUITS, CABLES, ETC., SHOWN ON PLANS ARE APPROXIMATE ONLY. IT IS NOT THE INTENT OF THESE PLANS TO SHOW THE EXACT LOCATION OF ALL UNDERGROUND UTILITIES AND STRUCTURES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LOCATIONS OF ALL EXISTING UTILITIES WITH THE RESPECTIVE OWNERS. EXISTING UTILITIES DAMAGED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT HIS OWN COST.
- THE LOCATIONS OF THE TRAFFIC SIGNAL STANDARDS, TRAFFIC SIGNAL STANDARDS WITH MAST-ARM, TRAFFIC CONTROLLER, PULL BOXES, CONDUITS AND TRAFFIC LOOP DETECTORS SHALL BE STAKED OUT IN THE FIELD BY THE CONTRACTOR AND APPROVAL OF THE LOCATIONS OBTAINED FROM THE ENGINEER PRIOR TO CONSTRUCTION AND INSTALLATION.
- ALL TRAFFIC SIGNAL WORK SHALL CONFORM TO THE REQUIREMENTS OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", FEDERAL HIGHWAY ADMINISTRATION (1978) AND AMENDMENTS.
- DEPARTMENT OF TRANSPORTATION SERVICES, CITY AND COUNTY OF HONOLULU, WILL ASSIST THE ENGINEER IN CONSTRUCTION INSPECTION FOR TRAFFIC SIGNAL SYSTEM.
- WORK BY THE DEPARTMENT OF TRANSPORTATION SERVICES, CITY AND COUNTY OF HONOLULU:
 - TEST CONTROLLER AND AUXILIARY EQUIPMENT IN CABINET.
 - MAKE ALL ELECTRICAL CONNECTIONS IN THE FIELD FOR SIGNAL SYSTEM AFTER THE SYSTEM HAS BEEN INSTALLED IN PLACE BY THE CONTRACTOR.
 - MASK AND ADJUST ALL PROGRAMMED VISIBILITY TRAFFIC SIGNAL HEADS.
 - FINAL ADJUSTMENT OF TRAFFIC SIGNAL CONTROL EQUIPMENT.
- REMOVAL OF EXISTING SIGNS SHALL ALSO INCLUDE THE REMOVAL OF POSTS AND FOUNDATIONS UNLESS OTHERWISE NOTED. ALL SUCH MATERIALS SHALL BE THE PROPERTY OF THE CONTRACTOR. COSTS SHALL BE INCIDENTAL TO OTHER ITEMS OF WORK.
- LOCATIONS OF TRAFFIC MARKINGS AND MARKERS (LANE LINES, STOP LINES, ETC.) SHOWN ON THE PLANS SHALL BE VERIFIED BY THE CONTRACTOR WITH THE ENGINEER PRIOR TO THE INSTALLATION OF THE TRAFFIC SIGNAL SYSTEM.
- REMOVAL OF PAVEMENT MARKINGS AND STRIPINGS SHALL BE DONE BY THE CONTRACTOR, THE COST OF SUCH WORK SHALL BE INCIDENTAL TO OTHER ITEMS OF WORK.
- A SOLID #8 BARE COPPER WIRE SHALL BE PULLED WITH THE 26C #14 CONTROL CABLE FOR EQUIPMENT GROUND. COST SHALL BE INCIDENTAL TO THE INSTALLATION OF THE CONTROL CABLE.
- STOP SIGNS INDICATED TO BE REMOVED SHALL BE REMOVED AFTER THE TRAFFIC SIGNAL SYSTEM IS OPERATIONAL.
- MAINTENANCE OF TRAFFIC THROUGH THE CONSTRUCTION AREA SHALL BE IN ACCORDANCE WITH PART VI OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", FEDERAL HIGHWAY ADMINISTRATION (1978). THE CONTRACTOR SHALL FURNISH AND MAINTAIN ADEQUATE BARRICADES, BLINKERS, CONSTRUCTION SIGNS, ETC. FOR THE SAFETY OF THE MOTORING PUBLIC. (SEE SPECIAL PROVISION FOR TRAFFIC CONTROL PLAN).
- SAW CUT EXISTING A.C. PAVEMENT, CURB AND CONCRETE ISLAND WHERE NEW CONSTRUCTION IS TO ABUT EXISTING, TO PROVIDE CLEAN NEAT APPEARANCE.
- CONTRACTOR SHALL NEATLY PATCH ALL AREAS WHERE EXISTING A.C. EDGES ABUTTING NEW WORK ARE BROKEN OR RAVELLING.

LEGEND

- ***** EXISTING STRIPING, PAVEMENT MARKERS OR CURB TO BE REMOVED
- NEW A.C. PAVEMENT
- [47.00] EXISTING ELEVATION
- [47.00] FINISH ELEVATION
- o.T.S. NEW TRAFFIC SIGNAL STANDARD

STATE OF HAWAII

DEPARTMENT OF TRANSPORTATION

HIGHWAYS DIVISION

LIKELIKE HIGHWAY

TRAFFIC SIGNALS AT

KALIHI STREET

PROJECT NO. 63A-01-80

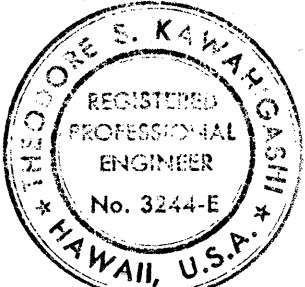
STRIPING PLAN

AND DETAILS

SCALE: AS SHOWN

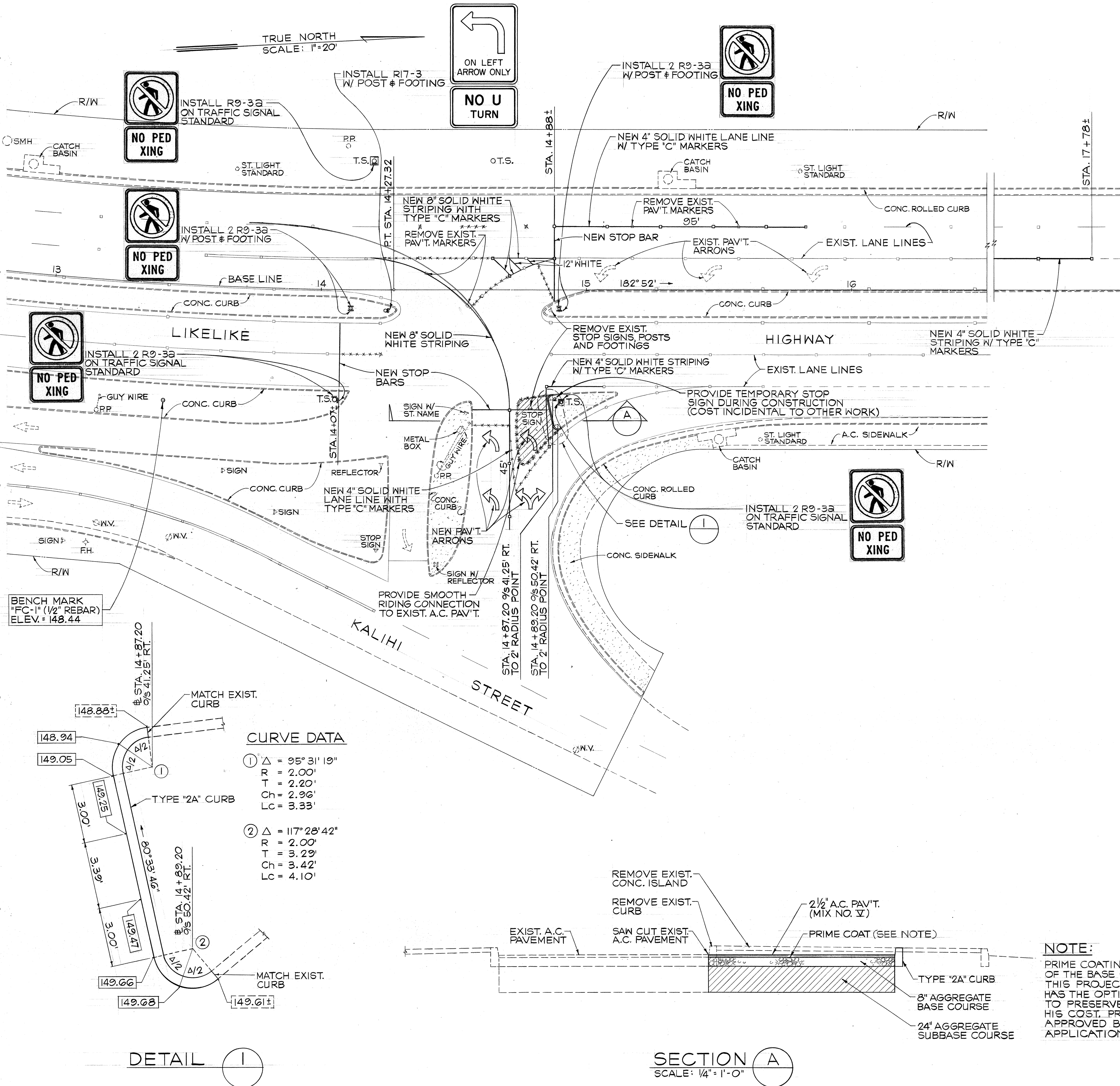
DATE: 8-1-79

SHEET No. 1 OF 1 SHEETS



THIS WORK WAS PREPARED BY ME
OR UNDER MY SUPERVISION
Harold S. Kawachi
Signature

NOTE:
PRIME COATING OF THE PREPARED SURFACE OF THE BASE COURSE IS NOT REQUIRED ON THIS PROJECT. HOWEVER, THE CONTRACTOR HAS THE OPTION OF APPLYING A PRIME COAT TO PRESERVE THE PREPARED SURFACE AT HIS COST. PRIME COAT MATERIAL SHALL BE APPROVED BY THE ENGINEER PRIOR TO APPLICATION.



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