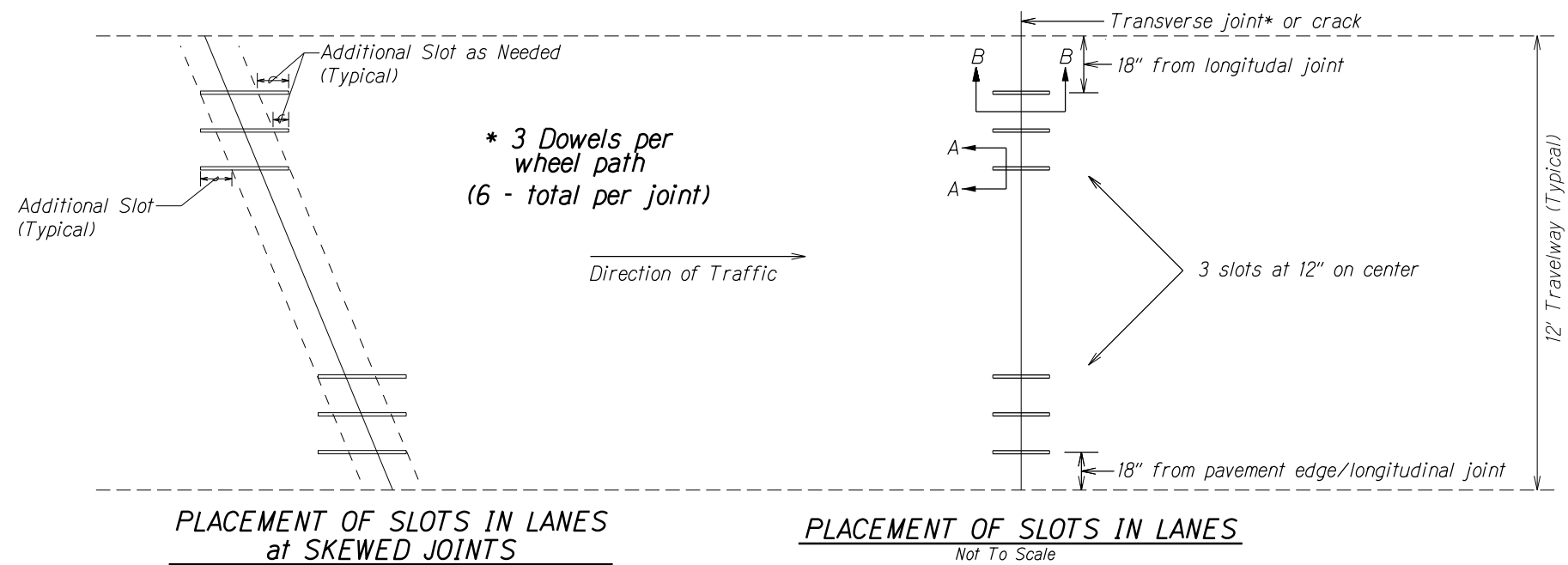
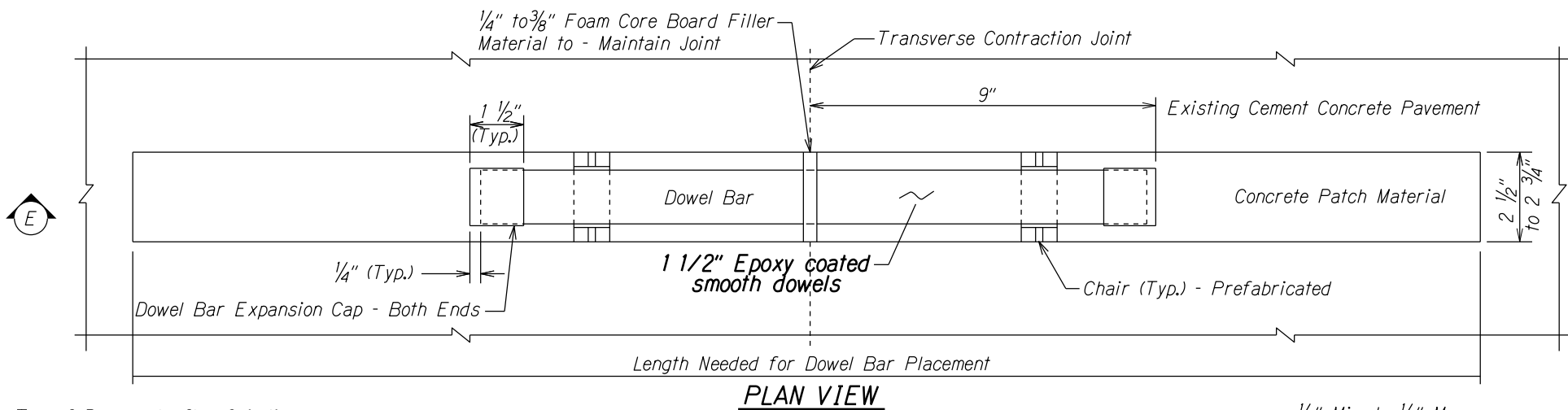


FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS



**\*Note:**

1. Slots may be offset 3 inches along the joint or shrinkage crack as needed to avoid other existing cracks or conditions that would affect dowel bar retrofit.
2. Cracks that are not coincidental with the construction joints (Volunteer cracks) near saw cut joints: Engineer has the option to retrofit either the saw cut joint or volunteer crack.

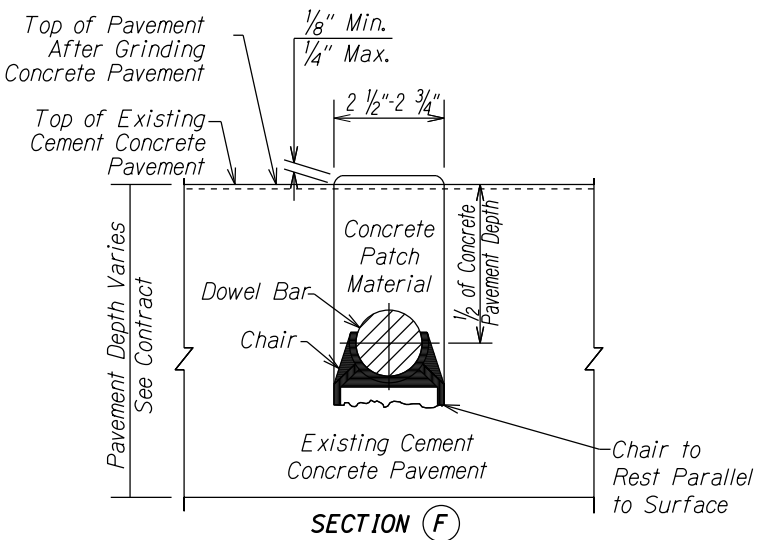
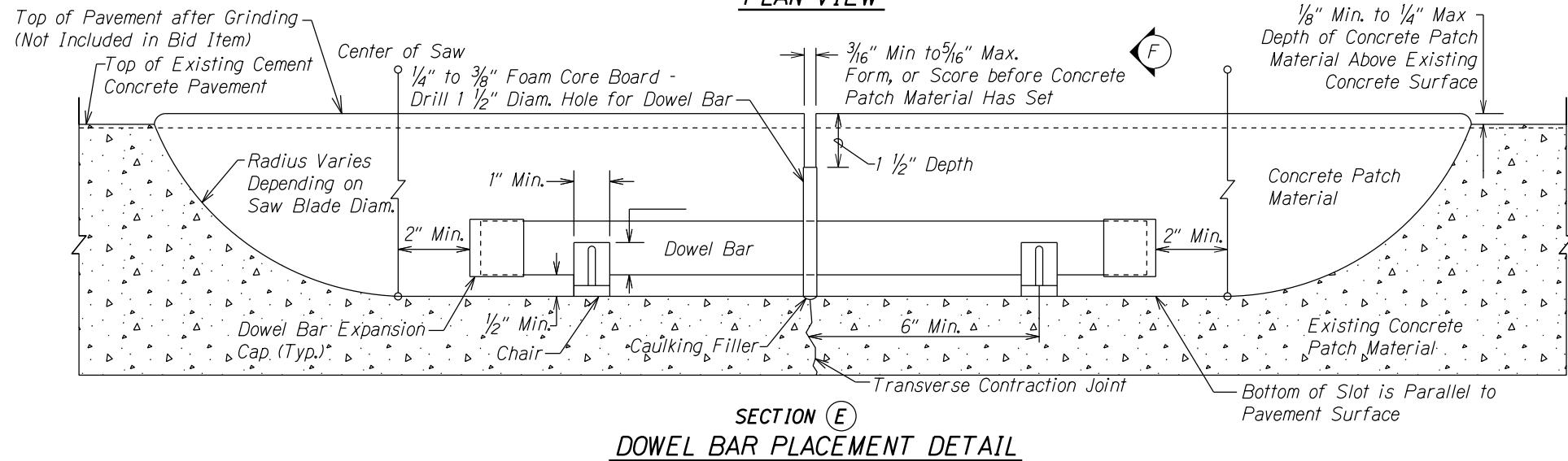


**NOTE:**

1. Direction and Angle of Skewed Joints May Not be as Shown.
2. Due to existing conditions (cracks, spalls, etc.), placement of the slot may be adjusted by the Engineer.

**NOTE:**

Width of foam core board to match existing transverse joint (width varies).



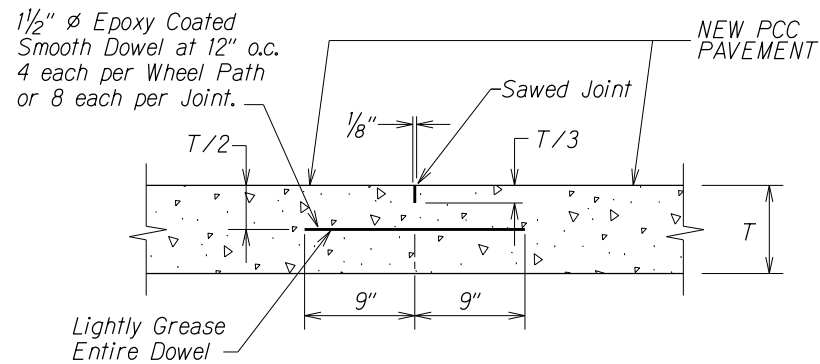
STATE OF HAWAII  
DEPARTMENT OF TRANSPORTATION  
HIGHWAYS DIVISION

**TYPICAL SECTION**

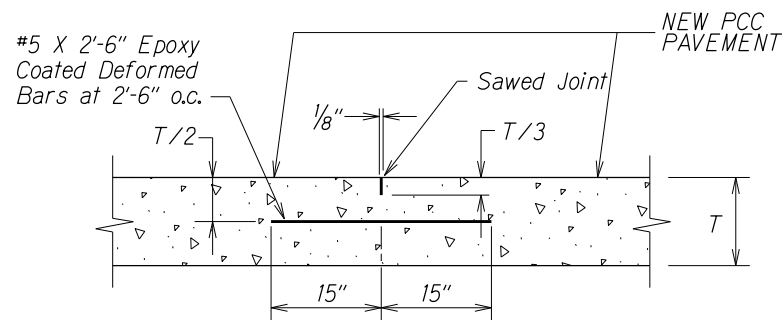
STP-0300(185)  
FIGURE 1

SHEET No. 1 OF 4 SHEETS

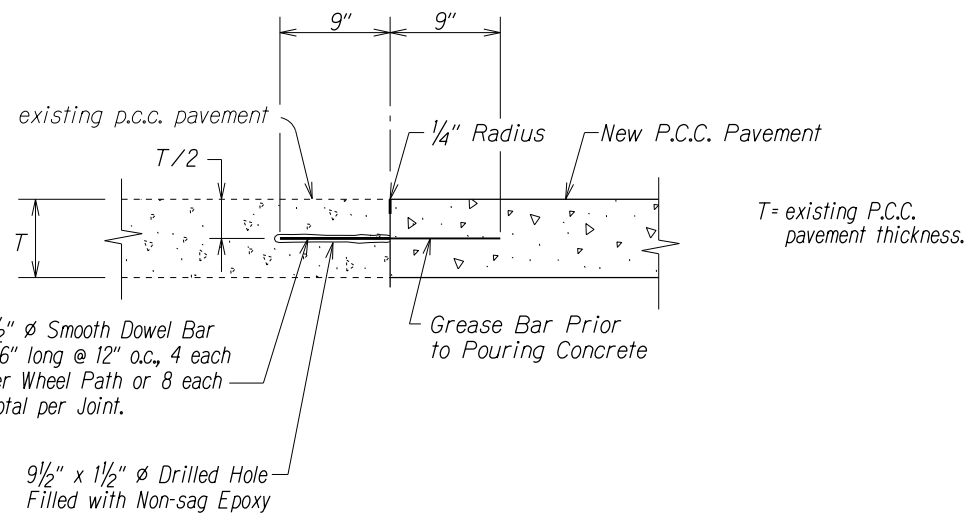
FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS



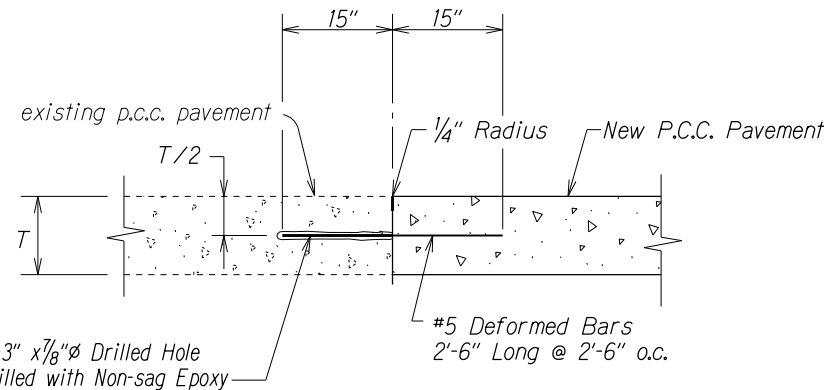
**TRANSVERSE CONTRACTION JOINT** (D-1)  
Not to Scale



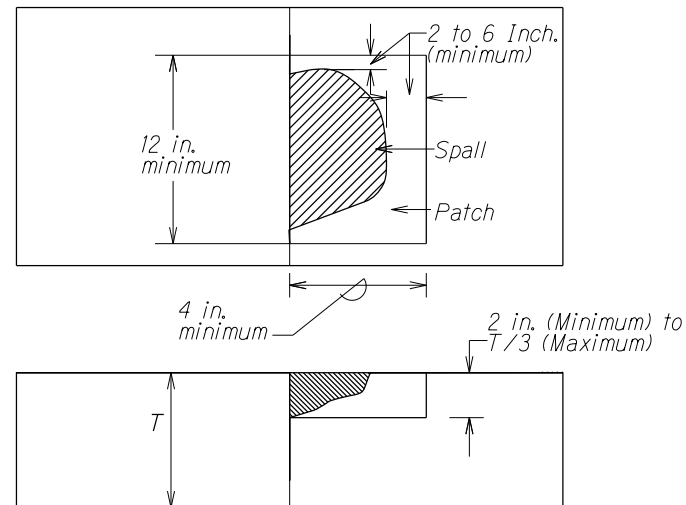
**LONGITUDINAL CONTRACTION JOINT** (D-2)  
Not to Scale



**TRANSVERSE CONSTRUCTION JOINT AT EXISTING P.C.C. PAVEMENT** (D-3)  
Not to Scale



**LONGITUDINAL CONSTRUCTION JOINT AT EXISTING P.C.C. PAVEMENT** (D-4)  
Not to Scale



**PARTIAL DEPTH SPALL REPAIR DETAILS**  
Not to Scale

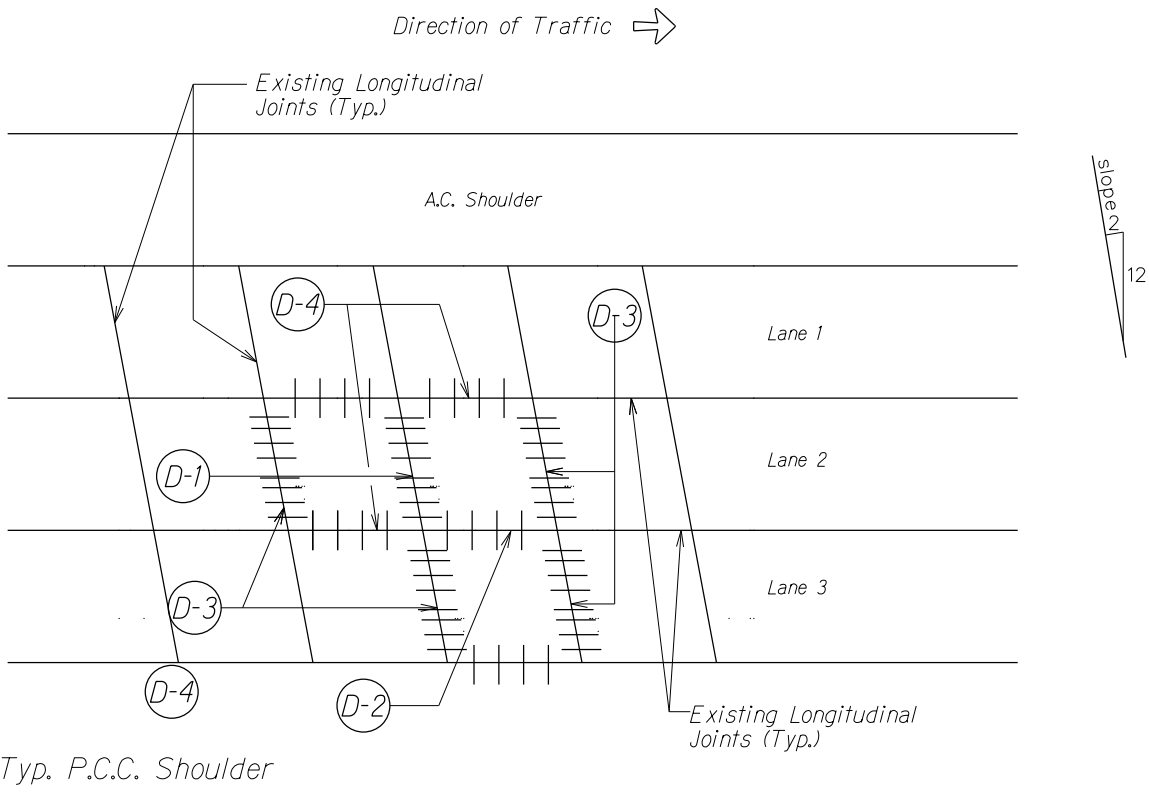
**NOTES:**

- Epoxy Coated Dowels and Deformed Bars shall Conform to AASHTO M284/M 284/M-95 Grade or 60.
- Transverse Construction Joints shall be located at a Minimum Distance of 10 Feet from the Nearest Transverse Contraction Joint.
- The dowels shall be positioned parallel to centerline direction of traffic and within the plane of the roadway surface. The ends of the dowels shall not deviate more than 0.25" from the parallel in 9" length.
- The Contractor shall not damage the epoxy coating on the dowels and deformed bars in any way during shipment, handling, or placement. Damaged epoxy coated dowels shall be replaced at no cost to the State.
- Distance dowels and deformed bars are to be located from a Longitudinal or Transverse Joint is 18 inches. Deformed Bars closer than 15 inches to the Transverse Joint can interfere with Joint Movement.
- The Contractor shall stake out the P.C.C. Slab to be removed to the nearest transverse joint and verify location with the Engineer.
- The Contractor shall saw cut the existing joint prior to excavation.
- POSSIBLE REPAIR AREAS:** (A. H-3)
  - Areas of broken PCC on H-3 (Halawa to Likelike):
    - Saw-cut and remove existing broken 9.5" PCC.
    - Recompact disturbed exposed Permeable base with jumping jack compactors.
    - Install tie bars and dowel bars.
    - Pour 9.5-inch thick PCC reinforced with structural fibers.
  - Areas of broken PCC on H-1 (small isolated areas Ewa to Kapolei): Same repair for H-2 (small isolated areas from H-1/H-2 interchange to Wahiawa)
    - Saw-cut and remove existing broken 9" PCC and underlying 4" CTB.
    - Recompact disturbed exposed Aggregate Subbase with jumping jack compactors.
    - Install tie bars and dowel bars.
    - Pour 13-inch thick PCC.

SURVEY PLOTTED BY	DATE
DRAWN BY	
DESIGNED BY	
QUANTITIES BY	
CHECKED BY	
ORIGINAL PLAN	
NOTE BOOK	
No.	

Notes: (Continued)

- C. Areas that are currently AC on H-1 (such as Inbound right shoulder across Sears Distribution Center):
- 1) Cold plane 11 inches.
  - 2) Recompact disturbed exposed Aggregate Subbase with jumping jack compactors.
  - 3) Pour 13-inch thick PCC.
- D. Areas that are currently UTW on H-1 (like off-ramp to Halawa Valley outbound and Aloha Stadium loop ramp inbound):
- 1) Saw cut 10 inches deep at the distressed UTW.
  - 2) Recompact disturbed exposed Aggregate Subbase with jumping jack compactors.
  - 3) Pour 10-inch thick PCC.
- E. Middle Street:
- 1) Redo curb and gutters for entire length.
  - 2) Remove and replace bad PCC.
  - 3) Diamond grind entire pavement to achieve a smoother pavement.



**FULL DEPTH SLAB RECONSTRUCTION**

*Not at Scale*

(Note: A.C. Shoulder - Form is Required and isolate (no tie-bars); Also, see notes for quantity of dowels, and tie bars required per joint.)

ORIGINAL PLAN	SURVEY PLOTTED BY	DATE
NOTE BOOK	DRAWN BY	
N.	DESIGNED BY	
	QUANTITIES BY	
	CHECKED BY	

FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS

STATE OF HAWAII  
DEPARTMENT OF TRANSPORTATION  
HIGHWAYS DIVISION

**ROADWAY CONSTRUCTION**  
**JOINT DETAILS**  
*STP-0300(185)*  
FIGURE 3

ORIGINAL PLAN NOTE BOOK add No. 11/10/01	SURVEY PLOTTED BY _____	DATE _____
	DRAWN BY <i>X</i>	_____
	DESIGNED BY <i>X</i>	_____
	QUANTITIES BY _____	_____
	CHECKED BY _____	_____

GENERAL NOTES

1. The scope of work for this project consists of slot saw cutting, concrete removal, slot cleaning and preparation, sealing joints and cracks in slot, placing dowel assembly in slot, mixing and placing backfill material, diamond grinding, pavement cracks repair and installation of pavement markings. This work may also include full and partial depth slab repair.

2. The Contractor's attention is directed to the following Sections of the Special Provisions : Subsection 107.13 - Public Convenience and Safety; Subsection 107.21 - Contractor's Responsibility For Utility Property And Services; and Section 645 -Traffic Control.

3. At the end of each day's work, the Contractor shall remove all equipment and other obstructions to permit free and safe passage of public traffic.

4. The encountering of underground utilities and other obstacles during the course of work is possible. The Contractor shall be held liable for any damages incurred to the existing facilities and/or improvements as a result of his operations.

5. The Contractor shall notify in writing, the Oahu Transit Services, Inc. Roads Supervision Office, 811 Middle St., Hon., HI 96819 (ph. #848-4571) seven (7) days prior to any paving operations.

6. The Contractor shall notify the Engineer in writing, two (2) weeks prior to starting paving operations.

7. Existing drainage system will be functional at all times during construction. The Contractor is to furnish materials, equipment, labor, tools and incidentals necessary to maintain flow. This work shall be considered incidental to various contract items.

8. The Contractor shall provide for access to and from all existing side streets at all times.

9. All saw cutting work and removal of slurry during saw cutting shall be considered incidental to Dowel Bar Retrofit bid items and shall not be paid for separately.

10. No material and/or equipment shall be stockpiled or otherwise stored within the highway right-of-way except at locations designated in writing and approved by the Engineer. If use of location is approved by the Engineer, the Contractor shall obtain a permit to use the property within the highway right-of-way from the State Right-Of-Way Branch at telephone no. 692-7332.

11. All saw cut/removed concrete and excess materials not needed for this project shall become the property of the Contractor. If disposed of, it shall be in accordance with the Environmental Protection Agency's policies and guide-lines. This work shall be considered incidental to the various contract items and will not be paid for separately.
12. Location of existing roadway uncontrolled shrinkage cracks which generally occurs at mid slab is not shown on the plans. Contractor shall inspect, locate, measure and map out all uncontrolled mid slab shrinkage cracks within project limits prior to start of work. Longitudinal or other cracks such as that caused by settling etc. are not within the scope of this work and shall not be retrofitted. Contractor shall submit the map of all uncontrolled mid slab shrinkage cracks to the State for review and allow the State 15 working days to determine which cracks to be retrofitted.

13. The Contractor shall develop and submit a safety and dust control plan to the State for approval prior to construction. The plan shall include a device capable of shielding all debris and dust from entering the adjacent travel lanes. This work shall not be paid for separately but shall be considered incidental to various contract items.

14. Layout of pavement markings and striping shall be done by the Contractor and approved by the Engineer prior to any installation work.

15. All existing pavement striping damaged by Contractor's operations shall be repaired by the Contractor at no cost to the State.

16. All pavement striping repairs shall be done in accordance with manufacturer's recommendations.

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GENERAL NOTES

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FIGURE 4

SHEET No. 4 OF 4 SHEETS