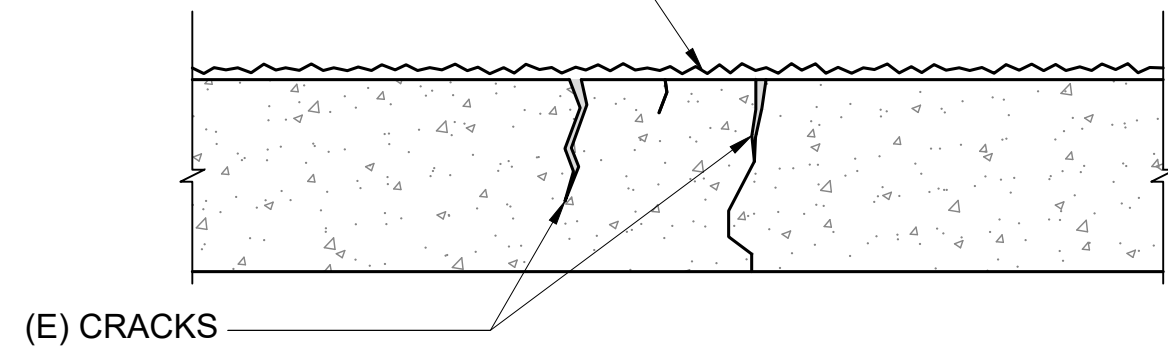


CRACK REPAIR NOTES:

1. DRAINS AND GRATES SHALL BE ADEQUATELY ISOLATED UTILIZING CONSTRUCTION PAPER TO PREVENT ANY PENETRATING SEALER FROM ENTERING DRAINAGE AND JOINT SYSTEMS. THE CONSTRUCTION PAPER WILL THEN BE REMOVED PRIOR TO FULL CURE OF THE EPOXY.
2. THE PENETRATING SEALER WILL BE APPLIED UTILIZING AN EPOXY PUMP SYSTEM SPECIALLY DESIGNED FOR EPOXIES. THE EPOXY PUMP IS MOUNTED ON THE REAR OF A TRUCK WHICH ALSO HOUSES TWO 250 GALLON TOTES OF MATERIAL. THE TOTES TO BE USED WILL BE BETWEEN 65°- 85°F. THE PUMP IS EQUIPPED WITH A DETAILED DIGITAL READ OUT DISPLAYING THE GALLONS PER MINUTE AND TOTAL VOLUME OF BOTH PART A AND B. IT ALSO GIVES THE TOTAL VOLUME OF MATERIAL PUMPED.
3. THE MIXED EPOXY WILL BE DISPENSED ONTO THE CONCOURSE DECK SURFACE, THIS WILL BE FOLLOWED BY A GROUND LABORER(S) USING A SAW-TOOTHED/NOTCHED SQUEEGEE TO SPREAD THE SEALER MEETING THE MANUFACTURER'S WET THICKNESS. ANY VARIATION IN THE SURFACE (I.E., CRACKS) SURFACE CONDITIONS WILL RESULT IN PENETRATING SEALER BEING SPREAD UNTIL REFUSAL.
4. FOLLOWING 5 MINUTES OF THE MATERIAL BEING WORKED INTO THE SURFACE, THE TOPPING AGGREGATE / SAND APPLICATION WILL START. ADJACENT TO THE SEALER APPLICATION WILL BE A TRUCK WITH A BULK SAND POT WITH THE TOPPING SAND. SPECIAL CARE WILL BE GIVEN AS TO NOT DIRECT THE HOSE DIRECTLY TO THE SURFACE DISRUPTING OR CAUSING THE EPOXY MATERIAL TO DISPERSE. SPECIAL CARE WILL ALSO BE TAKEN TO MINIMIZE DUST DURING THE PROCESS. DOT-A WILL APPROVE THE METHOD.
5. HAND BROADCASTING OF THE TOPPING SAND IS ALLOWED PROVIDED THE SAND CAN BE APPLIED OVER THE WET RESIN WITHIN 10 MINUTES OF THE RESIN HAVING BEEN PLACED. SPIKE SHOES WILL NEED TO BE WORN BY ANY PERSON WHO MAY COME IN CONTACT WITH WET PLACED SEALER WITHOUT AGGREGATE.
6. THE TOPPING SAND WILL BE APPLIED TO THE POINT OF REFUSAL. TO ENSURE NO WET SPOTS EXIST, A FINAL WALK OF THE SURFACE WILL BE DONE IMMEDIATELY BEHIND THE SAND APPLICATION TO ENSURE NO WET AREAS OF EPOXY SHINING THROUGH ARE PRESENT. IN THE CASE WET AREAS ARE VISIBLE, ADDITIONAL SAND WILL BE APPLIED TO THESE AREAS MANUALLY TO THE POINT OF REFUSAL.
7. ONCE ALL MATERIAL HAS CURED AND INSPECTED, A FULL SWEEP/VACUUM WILL COMMENCE PRIOR TO OPENING TO TRAFFIC.

CRACK PENETRATING SEALER
WITH TOPPING SAND

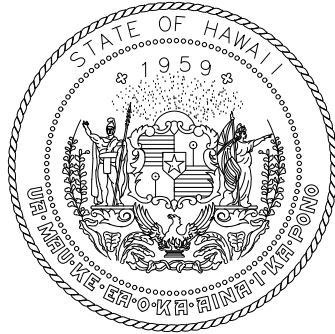


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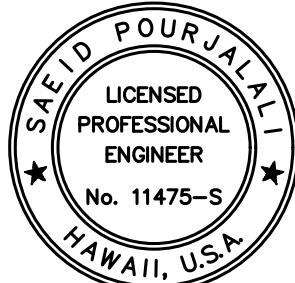
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CONCRETE STRUCTURAL HORIZONTAL CRACK REPAIR DETAIL

NOT TO SCALE



Airports Division
DEPARTMENT OF TRANSPORTATION
STATE OF HAWAII



Saad Pourjaleeli
04/30/2024

Licensed Expiration Date

This work was prepared by me or under my supervision

DSGN.	DRWN.	CHKD.	APPD.
MG	MG	SP	

KEY PLAN / NOTES:



10/20/22

ADDENDUM 1

NO.	DATE	REVISIONS
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CONSTRUCTION
DOCUMENTS

SEPTEMBER, 2022
DATE

PROJECT TITLE :

EWA AND DH CONCOURSE
ROADWAY IMPROVEMENTS
PHASE 1

AT
DANIEL K. INOUE INTERNATIONAL AIRPORT
HONOLULU, OAHU, HAWAII

PROJECT NO.:

AO1043-32

SHEET TITLE:

TYPICAL CRACK
REPAIR DETAILS

DATE :

SEPTEMBER, 2022

SHEET :

25 OF 247 SHEETS

DWG. NO.

S-202