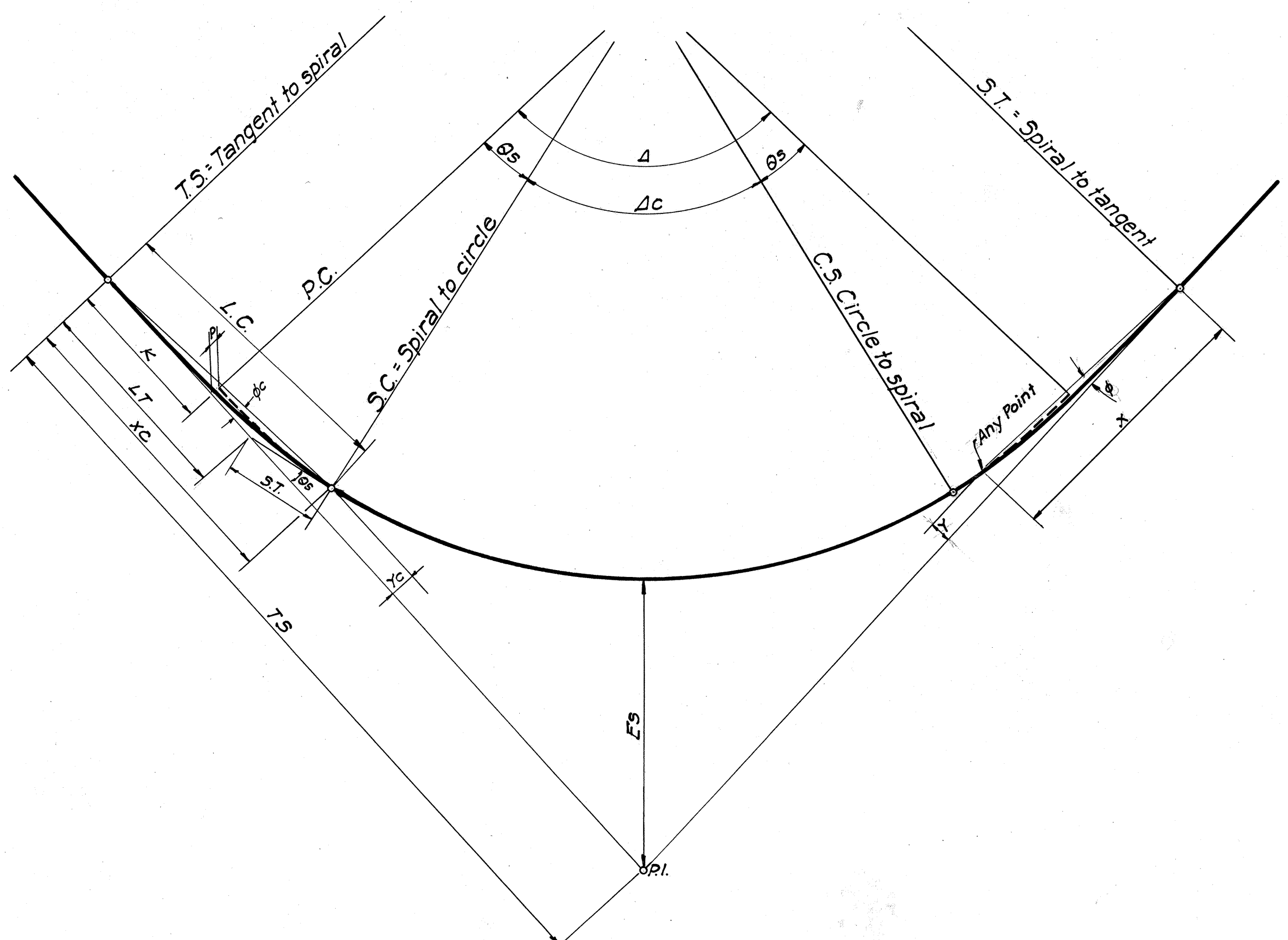


FED. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	F 32(2)	1946	2	27

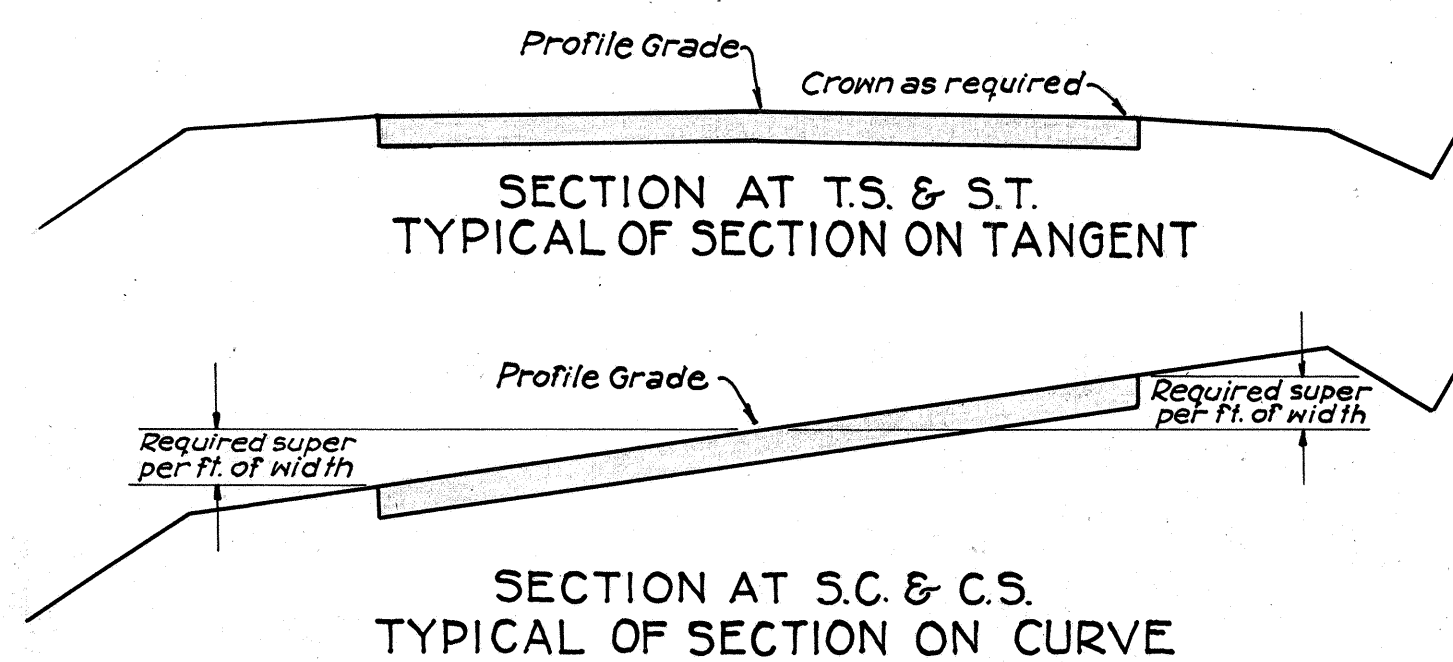
**SUPERELEVATION:-**  
 Superelevation shall be in accordance with the requirements for a designed speed of 50 miles per hour as recommended by the Public Roads Administration.  
 For simple curves superelevation shall start at a point 200' from the P.C. and reach the full super required at the P.C. by lowering or raising the outer edges of the pav't at a uniform rate from the standard crown to the required superelevation.  
 For spiral curves superelevation shall start at the T.S. and reach the full super required at the S.C. by lowering and raising the outer edges of the pav't at a uniform rate from the standard crown to the required superelevation.

**EXTRA WIDENING:-**  
 Widening shall be in accordance with the requirements for the designed speed of 50 miles per hour as recommended by the Public Roads Administration.  
 Widening shall begin at the T.S. with zero and attain the maximum at the S.C. and diminish from the maximum at the C.S. to zero at the S.T. Appearance of break at the S.C. and C.S. shall be avoided by eye adjustment of the stakes in the field.



**TYPICAL TEN-CHORD SPIRAL**

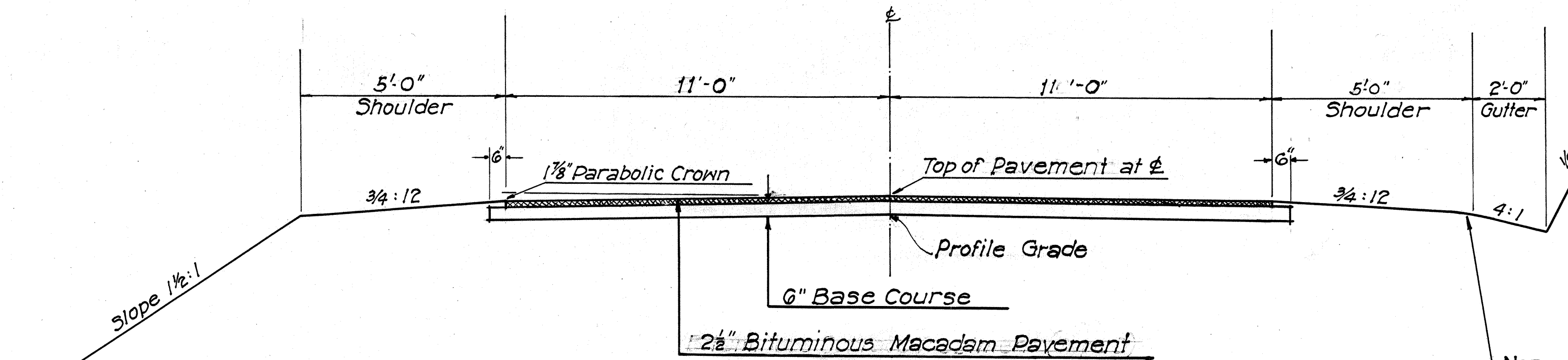
NOTE! Values for lettered dimensions to be taken from tables of spiral selected.



**BASE COURSE:** 6" Water Bound Macadam Base Course

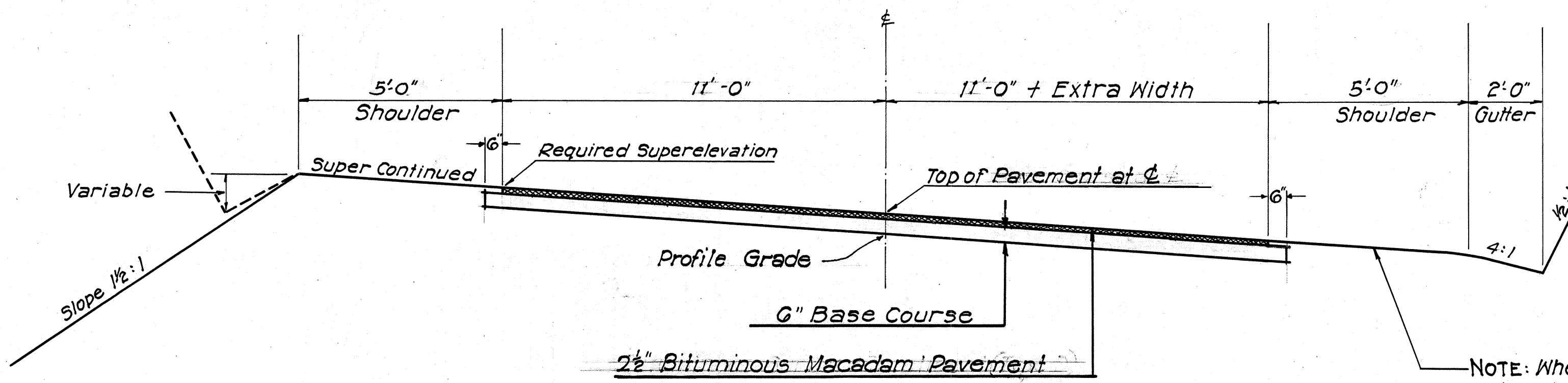
**PAVEMENT:** Shall be a Bituminous Macadam Pavement with a 2 1/2" finished or compacted thickness all as follows: 2 1/2" (compacted) coarse rock (2-1"); 1.25 gals. per sq. yd. of Bituminous Binder (85-100); approximately 0.01 cu. yd. per sq. yd. of Keystone (3/4" - #4); 0.40 gal. per sq. yd. of Bituminous Binder, approximately 0.005 cu. yd. per sq. yd. of chips (3/8" - #10). Seal coat - 0.20 gal. of Bituminous Binder covered with 0.003 cu. yd. per sq. yd. of rock sand (#4 - #20).

**GENERAL NOTES:** Particular reference is made to the specifications and special provisions covering these item of work.



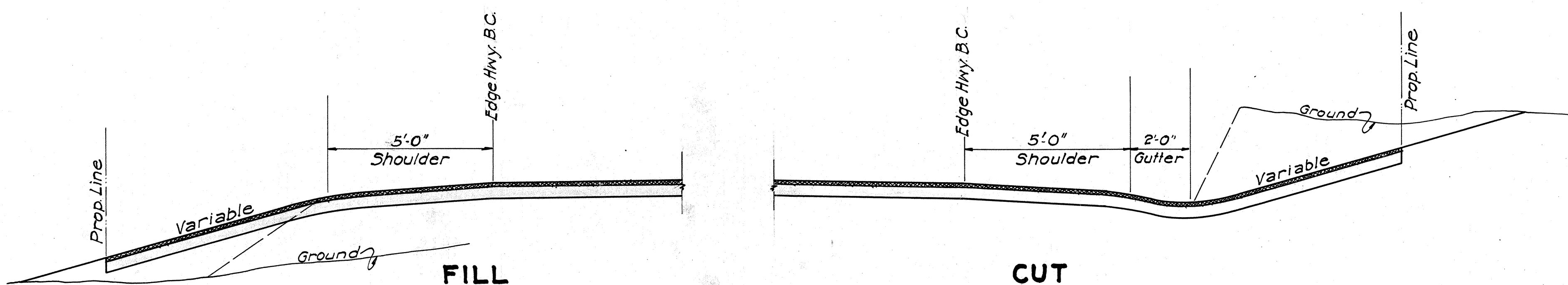
**TYPICAL ROAD SECTION ON TANGENTS**  
 Scale: 3/8" = 1'-0"

NOTE: In cut sections the shoulders and gutters shall be formed to the dimensions and slopes required and then the edge of shoulders rounded off evenly at no particular radius.



**TYPICAL ROAD SECTION ON CURVES**  
 Scale: 3/8" = 1'-0"

NOTE: When super is less than .063 ft. per ft. shoulder slope is 3/4:12. When super is equal to or greater than .063 ft. per ft. shoulder slope is the same as the rate of super. This note applies only to the shoulder on the lower side of the super.



**DETAILS OF PAVED SIDE ROAD APPROACHES**  
 Scale: 3/8" = 1'-0"

TERRITORIAL HIGHWAY DEPARTMENT  
TERRITORY OF HAWAII

**TYPICAL SECTIONS**  
**HANA BELT ROAD**  
FEDERAL AID PROJ. F 32(2)  
JUNE, 1946

ORIGINAL	DATE
FILED	
BY	
FILED	
BY	
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