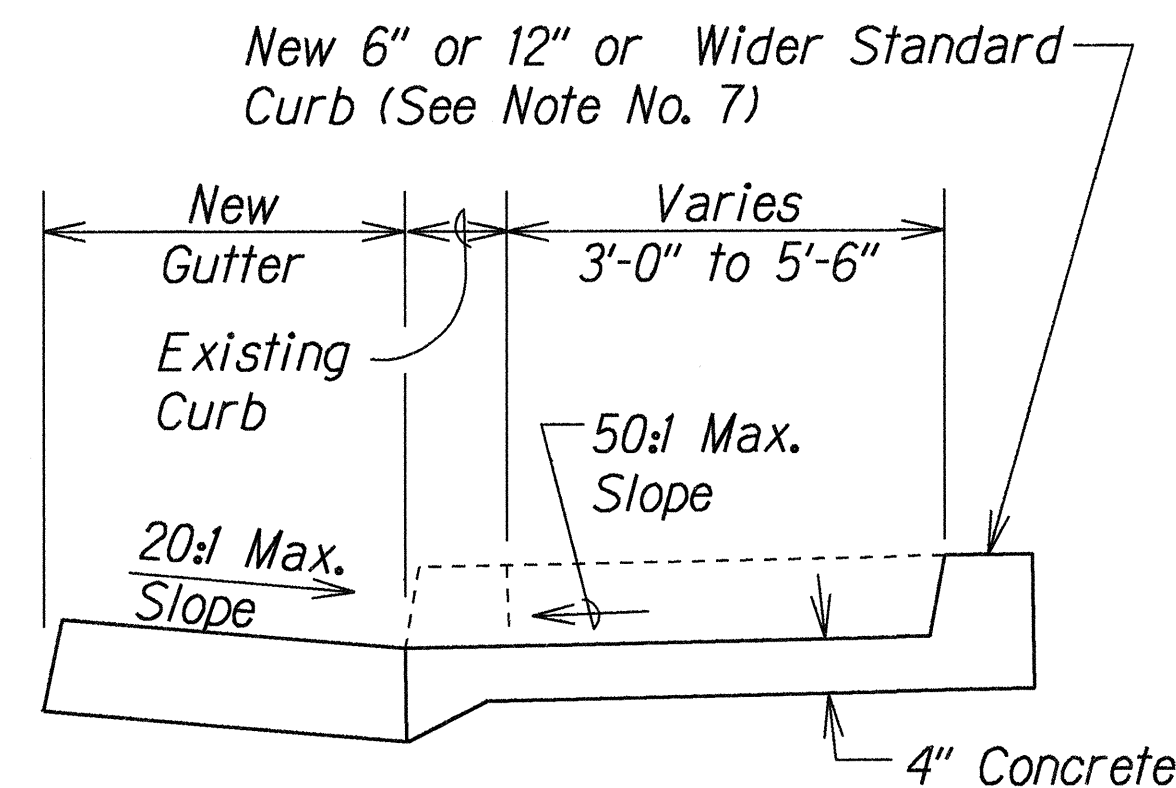


LEGEND

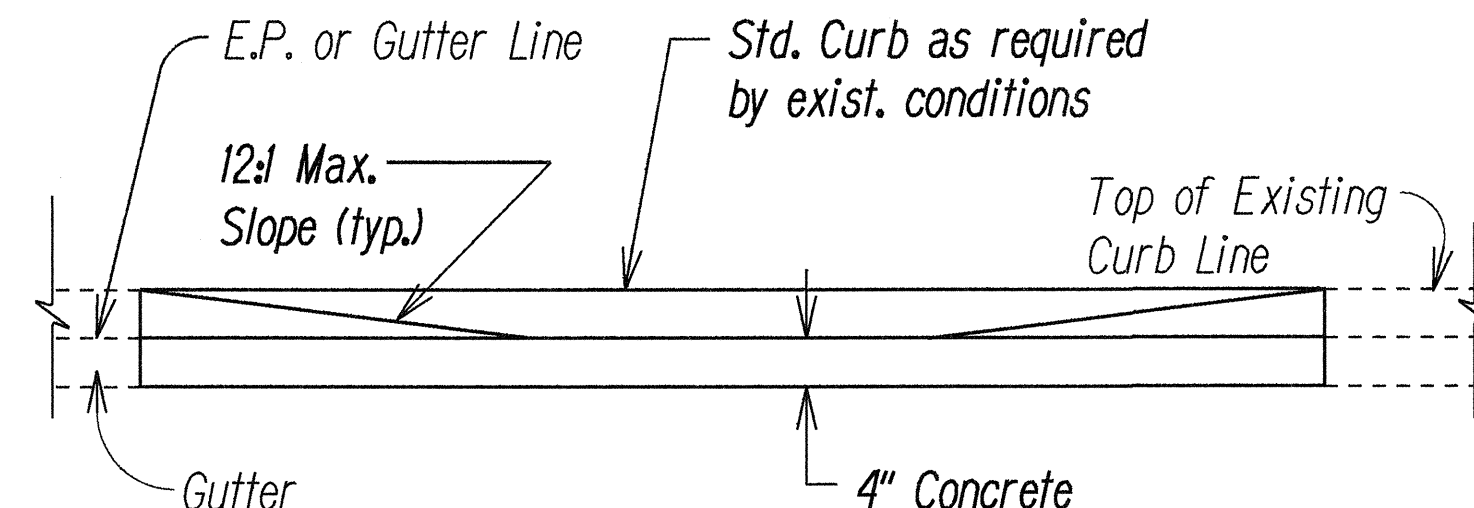
- 4 each Type A Raised Pavement Markers
Type C Raised Pavement Markers @ 40' a.c.
- 4 each Type J Raised Pavement Markers
Type D Raised Pavement Markers @ 40' a.c.
- 8" White Stripe with Type C Raised Pavement Markers @ 20' a.c. (Tape, Type I or Thermoplastic Extrusion)
- 4" Double Solid Yellow with Type D Raised Pavement Markers @ 20' a.c. (Tape, Type I or Thermoplastic Extrusion)
- 4" Double Solid Yellow Stripes with Type H Raised Pavement Markers @ 20' a.c. (Tape, Type II or Thermoplastic Extrusion)
- 4" Yellow Edge Stripe with Type H Raised Pavement Markers @ 40' a.c. (Tape, Type II or Thermoplastic Extrusion)
- 4" Double Solid White Stripes with Type C Raised Pavement Markers @ 20' a.c. (Tape, Type I or Thermoplastic Extrusion)
- Lane Change Restriction Marking
- 4 each Type A Raised Pavement Markers
Type C Raised Pavement Markers @ 20' a.c.
- 4" White Stripe (Tape, Type I or Thermoplastic Extrusion)
- 4" or 8" White Edge Stripe with Type C Raised Pavement Markers @ 40' a.c. (Tape, Type II or Thermoplastic Extrusion)
- 4" White Guide Lines (Tape, Type III or Thermoplastic Extrusion except for bus bays)
- 12" Yellow Transverse Median Marking (Tape, Type II or Thermoplastic Extrusion)
- 12" Wide White Transverse Shoulder Marking (Tape, Type II or Thermoplastic Extrusion)
- Channelizing Island or Deceleration Lane Gore (Tape, Type II or Thermoplastic Extrusion)
- Crosswalk and Stop Line. All Stop Lines shall be 10' from Crosswalk unless otherwise noted. Circled number indicates the number of lanes for payment (Tape, Type III or Thermoplastic Extrusion)
- Pavement Arrow (Tape, Type III or Thermoplastic Extrusion)
- Pavement Word (Tape, Type III or Thermoplastic Extrusion)
- 4 Each Type J Raised Pavement Markers
Type D Raised Pavement Markers @ 40' a.c.
Type H Raised Pavement Markers (Reflective Surface facing no-passing direction)
4" Single Solid Yellow Stripe (Tape, Type I or Thermoplastic Extrusion)
- Extension of Edge Line, 4" Wide x 2' Long White Stripe @ 10' a.c. with Type C Raised Pavement Markers @ 40' a.c. (Tape, Type III or Thermoplastic Extrusion)
- Reflector Marker (RM-2, White) with Post
- Reflector Marker (RM-2, Yellow) with Post
- Reflector Marker (RM-3) with Post
- Reflector Marker (RM-4, (L) or (R)) with Post

NOTES

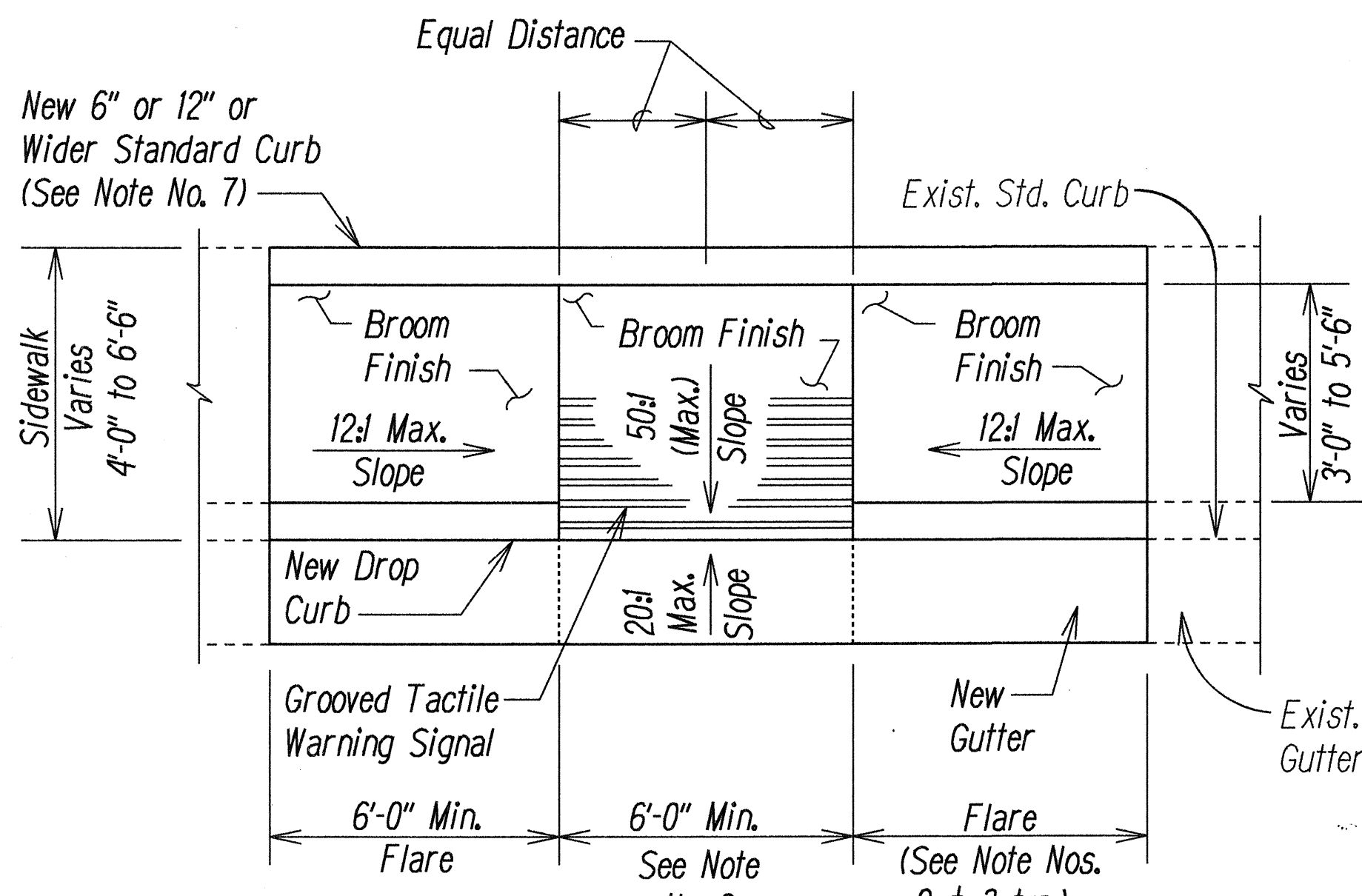
- Layout of pavement markings and striping shall be done by the Contractor and approved by the Engineer prior to any installation work.
- Existing pavement markings not incorporated in the final traffic pattern shall be removed as directed by the Engineer. The costs shall be incidental to the various pavement marking items.
- Raised pavement markers shall not be installed within crosswalks.
- Final locations of all signs shall be approved by the Engineer prior to any installation work.
- Existing signs not shown on these plans shall remain as posted unless otherwise directed by the Engineer. Removal and disposal of existing signs and/or posts as designated on these plans shall be incidental to the various signing items.
- Final locations of all stop lines shall be approved by the Engineer prior to installation.
- All pavement striping shall be as noted on the legend or plans.
- All preformed pavement marking tapes over existing pavement shall be applied with an approved primer as recommended by the tape's manufacturer and as approved by the Engineer. The primer shall be allowed to dry to the tacky stage prior to tape application.
- The Contractor shall erect advanced construction warning signs at the beginning and at the end of the project site. Construction warning signs shall be placed as indicated on the plans or as directed by the Engineer. The signs shall be kept in place for the duration of the project and shall be maintained by the Contractor. These signs shall be placed in addition to the required traffic control signs called for in Section 645-Traffic Control. The advanced construction warning signs shall be new and become the property of the State. The Contractor shall remove, clean and deliver the signs and posts to the Kauai District Baseyard or as directed by the Engineer of the project.
- Existing signs that are to be replaced shall not be removed until new signs are installed as replacements, or the messages are no longer necessary.
- Backing for all new regulatory and warning signs shall not be spliced.
- All sign panels shall conform to Section 621 of Special Provisions and the latest editions and amendments of the following FHWA publications:
 - "Manual on Uniform Traffic Control Devices for Streets and Highways" (MUTCD)
 - "Standard Highway Signs"
 - "Standard Alphabets for Highway Signs"
- State Route Marker Symbols, borders, messages, arrows, symbols and shields shall conform to details shown on the plans and as specified in the MUTCD.
- All new and relocated signs and markers installed on pipe posts, light standard or expressway sign post are to be mounted with band brackets and steel braces.
- Removal of existing delineators and posts, as directed by the Engineer or as shown on the plans, shall be considered incidental to the various signing items.
- Removal and reinstalling existing street name signs on regulatory and warning sign post shall not be paid for separately but shall be considered incidental to the various signing items.
- Existing pavement markers and striping between Sta. 41+07 and Sta. 46+85 shall be removed and re-striped as defined in the Pavement Marking & Signing Plan.
- Top and front face of existing sidewalk curb, between new "NO PARKING" signs and offset left from the the baseline, shall be painted red.



TYPICAL SECTION



ELEVATION



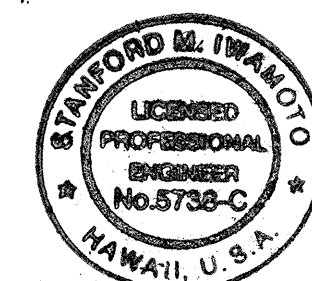
PLAN

CURB RAMP - TYPE "B"
SIDEWALK WIDTH 4'-0" OR GREATER BUT LESS THAN 7'-0" WIDTH
Not to Scale

| FED. ROAD DIST. NO. | STATE | PROJ. NO. | FISCAL YEAR | SHEET NO. | TOTAL SHEETS |
|---------------------|-------|-----------|-------------|-----------|--------------|
| HAWAII | HAW. | 56A-01-97 | 1998 | 6 | 8 |

CURB RAMP NOTES:

- The New Gutter fronting the curb ramps shall not exceed a slope of 20:1 maximum. Areas without a gutter shall be handled on a case by case basis in order to meet the 20:1 maximum slope requirement.
- The New Gutter fronting the 6'-0" minimum flares on both sides of the New Curb Ramps shall have a transition slope from 20:1 maximum to the existing gutter slope.
- When the curb exceeds the standard 6" height, the side flares and ramp may be greater than 6'-0" in order to meet the 12:1 maximum slope requirement.
- Curb Ramps shall be reinforced if adjacent sidewalks are reinforced.
- The Contractor shall use the Edge of Pavement (E.P.) fronting the new curb ramp or gutter as the control elevation when constructing the New Curb Ramp or Curb Ramp with gutter.
- For Type "B" & Type "E" Curb Ramps, New Standard Curb shall be 12" wide when installing Type I Traffic Signal Standard (TSS) or Pedestrian Push Button Pedestal (PPB). Where Type II or Type III TSS or Highway Light Pole is utilized, the width of the New Standard Curb shall be determined by the Engineer. For sidewalk less than 4'-0" (excluding curb), an Engineer approved 3'-0" wide minimum curb ramp shall be installed.
- The width of the curb ramp including curbs shall match the width of the sidewalk. When the sidewalk width is less than 4'-0", an Engineer approved, modified sidewalk with a minimum 3'-0" wide curb ramp excluding curbs shall be installed.
- The width of the curb ramp run shall be increased to 10'-0" at areas with high pedestrian traffic and as directed by the Engineer or as shown on the plans.
- A minimum 3'-0" wide by 5'-0" long (excluding curbs) flat, stable paved surface with a cross slope of 50:1 or flatter shall be provided for a parallel approach to the pedestrian push button.
- The pedestrian push button shall meet current American with Disabilities Act (ADA) Accessibility Guidelines and will be subject to Engineer's approval.
- All Pullboxes shall be installed within the sidewalk/unpaved area or as directed by the Engineer. Pullboxes shall not be install within the curb ramp (including flares).
- When necessitated by existing physical conditions, alternate Curb Ramps may be used subject to Engineer's approval.
- Subject to field conditions, the Engineer shall determine final location of Curb Ramp.
- Where necessary, existing pullboxes, handholes, manholes, etc. shall be adjusted to match Curb Ramp grade. Adjustments shall not be paid for separately but shall be considered incidental to the various wheelchair items.
- If possible, install utility poles, fire hydrants, light poles, sign posts, pullboxes, etc. off of sidewalk but within the right-of-way.
- Objects protruding from utility poles and walls adjacent to the sidewalks (i.e. wall mounted fire hydrants, telephones, meters on poles, etc.) shall be mounted to meet the current American with Disabilities Act (ADA) Accessibility Guidelines and will be subject to Engineer's approval.



THIS WORK WAS PREPARED BY ME OR UNDER MY SUPERVISION

Signature

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

**PAVEMENT MARKING LEGEND,
NOTES, & CURB RAMP DETAIL**

KUHIO HIGHWAY,
LEFT TURN STORAGE LANE TO
HAUAALA ROAD AND KAWAHAU ROAD
Proj. No. 56A-01-97

Scale: As Shown Date: May, 1998

SHEET No. / OF / SHEETS