

In 2010, monitoring of a fiber optic line along a short stretch of Kuhio Highway near Kuamoo Road and Wailua River Bridge was carried out by SCS (Dega and Powell 2010). This was a continuation of the fiber optic line first monitored by Dega and Powell (2003). Two fifty-foot linear trenches were excavated, and no archaeological material was encountered.

#### **Prior Studies Conducted within the Current APE**

In 2011, CSH conducted an archaeological assessment for the *makai* path of the Lydgate Park-Kapa'a Bike Path, which includes a portion of the current APE. The project entailed the development of a 10- to 12-foot-wide multi-use recreational path along a 2-mile stretch of coastline from Wailua to Kapa'a (Yucha and Hammatt 2011). No cultural resources were identified during the assessment. However, previous archaeological research indicated a high probability for Precontact subsurface cultural layers and human burials. CSH recommended an on-site archaeological monitoring program for all construction activities. In October 2014, CSH carried out the archaeological monitoring program and produced a report for a segment of the Kaua'i Bike/Pedestrian Path from Lydgate Park to Lihi Park, which includes a portion of the current project area (Kamai and Hammatt 2014). During the monitoring for Phase A, Segment 1 (Kuhio Highway north of Wailua River Cane Haul Bridge to the fork at Papaloa Road) no surface or subsurface cultural material and/or features were encountered.

HDOT's State Historic Bridge Inventory & Evaluation report (MKE and Fung 2013) documented two bridges (Wailua River Bridge and Wailua River Plantation Bridge) and the remnants of a third bridge (Wailua River Concrete Arch Bridge) at the mouth of the Wailua River. A portion of the Wailua River Plantation Bridge is within the current APE. Both the Wailua River Bridge and the remnants of the Wailua River Concrete Arch Bridge are outside of the current APE. The National and State Register eligible Wailua River Bridge was built in 1949 and replaced the 1919 Wailua River Concrete Arch Bridge, remnants of which are still present and were determined eligible by the SHPO's adoption of HDOT's State Historic Bridge Inventory & Evaluation report (MKE and Fung 2013). The Wailua River Plantation Bridge, also known as the Wailua Cane Haul Bridge was constructed in 1921 as a railroad bridge, the bridge was completely replaced by a new bridge in 2012 and, therefore, has lost integrity (MKE and Fung 2013) and is not considered a historic property.

In 2020, Mason Architects, Inc. prepared a National Register of Historic Places (NRHP) Registration Form for the former Coco Palms Resort (Westfall 2020). The resort was nominated for the NRHP and listed in the Hawaii Register of Historic Places (HRHP) as a district and included 25 buildings, two sites, a dozen structures, and approximately 50 objects. Weuwē-Kawai-Iki Fishpond, which was already listed in both the HRHP and NRHP in 2009, was considered a contributing resource for 2020 nomination. The main resort sits on a 32-acre parcel *mauka* of Kuhio Highway with "a small section of discontinuous land across the highway (*makai*) fronting the beach [which] contains the Seashell Restaurant" (Westfall 2020:4). The Seashell Restaurant is within the current APE (see Figure 10) and considered to be one of the contributing elements to the Coco Palms Resort Historic District. The resort has been non-operational since 1992 when Hurricane Iniki struck the island of Kaua'i resulting in severe damage to buildings and structures on the property. Although there has been severe deterioration to the existing buildings and structures, Westfall (2020:4) concluded that "Coco Palms retains integrity of location, setting, feeling, and association."

### **3. ANTICIPATED FINDINGS**

Wailua Ahupua'a was an area of great importance in Precontact times (Flores 1995). Wailua River was the center of business and governance used by high chiefs and chiefesses of Kaua'i. The *ahupua'a* contains numerous significant cultural sites and archaeological deposits, many of which are situated at the coast near the mouth of the River; however, there are no known historic properties associated with traditional Hawaiian use within the current APE. Archaeological excavations conducted in the vicinity of the APE along the *mauka* shoulder of Kuhio Highway (Hammatt and Shideler 2007) and archaeological monitoring (Dega and Powell 2003) revealed a very residual cultural deposit that is likely an extension of the intermittent buried cultural deposit (SIHP Site 1711) recorded by Buffum and Dega (2002) and Hoffman et al. (2005) on the Coco Palms Hotel property (Figure 11). Extensive archaeological work (Elmore and Kennedy 2000; Kikuchi 1973) on that same property also revealed numerous burials (see Figure 11). Given the culture-historical background and the findings of prior archaeological studies combined with the fact that the entirety of the APE lies within an area already developed for the highway and along the immediate shoreline and off-shore areas, although unlikely, anticipated findings are limited to a remnant buried cultural layer perhaps associated with Site 1711 and possible burial features. Additionally, one architectural resource (the Seashell Restaurant) considered a contributing element to the Coco Palms Resort Historic District is situated in the northern corner the current APE. This resource will be protected from impact by the placement of construction fencing prior to work commencing.

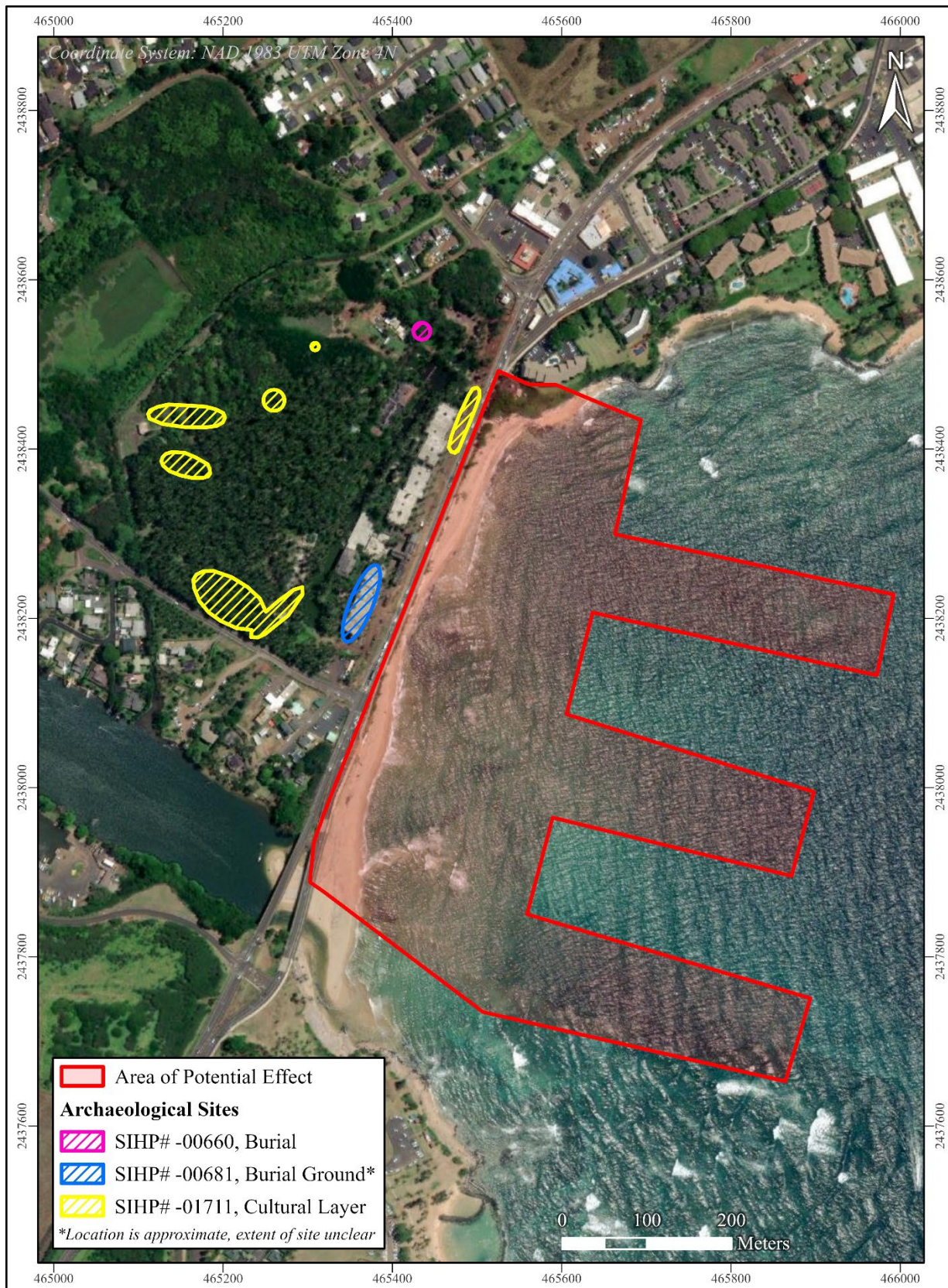


Figure 11. Locations of previously documented archaeological deposits in the vicinity of the current APE.