

GENERAL NOTES:

1. Scope of Work: The project consist of stabilizing shoulders and gutters, installing culvert and underdrain pipes, constructing and reconstructing grated drop inlets, removing and installing guardrails and reflector markers, edge pavement striping, and trimming and dressing of shoulders.
2. All lanes shall be open to traffic during the morning peak hours from 6:00 a.m. to 8:30 a.m. and during the afternoon peak hours from 3:30 p.m. to 6:00 p.m. and during off work hours. Only one lane of Highway shall be closed at any other time.
3. Minimum shoulder slope shall be 5%. When super slope is equal to or greater than 5% the shoulder slope shall be the same as the rate of super. This rate applies only to the lower side of the super. On high side, continue super-elevation of travelway.
4. Existing shoulders beyond paved shoulders shall be trimmed and dressed. Trimming and dressing of shoulders shall include but not limited to clearing, grubbing, backfilling, grading and compacting the shoulder area. This work shall not be paid for separately but shall be considered incidental to the various contract items.
5. 55-1 tack coat shall be applied at the longitudinal joint connecting the existing pavement to the new paved shoulder and over areas to be resurfaced. Measurements and payment for this item shall be considered incidental to "Item No. 401.0500."
6. Any damage to the existing pavement by the contractor shall be repaired at his expense and to the satisfaction of the Engineer.
7. No material or equipment shall be stockpiled or parked within the Highway Right-of-Way except at locations designated by the Engineer.
8. Contractor shall verify all utilities prior to commencement of any construction activities. Any damages incurred to existing utility facilities as a result of his operation shall be repaired and/or replaced immediately in accordance to the Standards and Specification of the State and/or the affected Utility Company. On any unreasonable delay by the Contractor, the State shall repair said damages and bill the Contractor.
9. Pavement striping under Project will be measured and paid for by the gallon.
10. All depressions and holes resulting from the removal of existing concrete posts shall be properly backfilled and compacted. Payment for this work shall be considered incidental to "Item No. 202.0420".
11. Removal of existing guardrail prior to installation of guardrail shall be permitted in increments as approved by the Engineer and no open section of guardrail shall be permitted after daily work ceases. All existing and new guardrail shall be functional during non-working hours.
12. The cost of removing wholly or portion of existing conc. headwall and drop intakes; structure excavation, concrete and reinforcing steel for grated drop inlets; and back filling of swale in the area of drainage improvements shall be considered incidental to various contract items of work.

13. All terminal sections except B.C.T. lengths are included in "Item No. 606.3110".
14. Existing ground line indicated in Typical Section Varies. Surface indicated is entirely imaginary and not intended to represent any specific location.

15. All existing guardrails with posts removed, including hardware shall remain the property of the State. The work necessary in removing and salvaging guardrails and posts at the State Baseyard in Kapaa as directed by the Engineer. This work will not be paid for separately, but shall be considered incidental to Item No. 606.3110.

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	HWY-K-01-87	1987	7	6

