

GENERAL PHASING NOTES:

1. UNLESS OTHERWISE INDICATED, ALL WORK SHALL BE PERFORMED BETWEEN THE HOURS SHOWN ON THESE CONSTRUCTION PHASING DRAWINGS.
2. THE CONTRACTOR'S ATTENTION IS DIRECTED TO ADVISORY CIRCULAR 150/5370-2G OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION AND THE CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), TO WHICH THE CONTRACTOR SHALL ADHERE TO.
3. A MINIMUM OF 3 HOURS PRIOR TO THE BEGINNING OF EACH WORK SHIFT, THE CONTRACTOR SHALL COORDINATE WITH AOC FOR GO/NO GO DECISION TO DETERMINE IF WORK WILL BE PERMITTED DURING THE UPCOMING SHIFT. DUE TO CERTAIN WEATHER CONDITIONS, AOC AND/OR ATC MAY CANCEL WORK SHIFTS AND REQUIRE THE OPENING OF THE RUNWAY AND TAXIWAYS FOR AIRCRAFT USE. WEATHER CONDITIONS WHICH WOULD REQUIRE CANCELLATION OF WORK TYPICALLY OCCUR DURING KONA WIND CONDITIONS WITH CERTAIN WIND VELOCITY THRESHOLDS. HISTORIC DATA FROM THE AIRPORT SHOWS THAT THESE CONDITIONS OCCUR LESS THAN 10% OF THE TIME. NOTE THAT THE AIRPORT WILL NOT CLOSE RWY 3-21 OR RWY 17-35 NIGHTLY OR BEGIN A NEW PHASE WITH CONTINUOUS CLOSURE OF RWY 3-21 IF KONA WINDS ARE PREDICTED.
4. LIGHT TOWERS SHALL BE PROVIDED BY THE CONTRACTOR FOR ALL NIGHTTIME WORK SPACED AT NOT GREATER THAN 100 FEET. LIGHT TOWERS SHALL BE SHIELDED AND DOWN POINTING LIGHTS, NOT BE AIMED SUCH THAT THEY INTERFERE WITH PILOT VISIBILITY OR TOWER PERSONNEL VISIBILITY. FURTHER RESTRICTIONS MAY BE REQUIRED DURING SHEARWATER BIRD SEASON FROM SEPTEMBER TO DECEMBER IF NIGHTTIME WORK IS PERMITTED.
5. AT THE BEGINNING OF EACH SHIFT/NIGHT, CONTRACTOR SHALL COORDINATE THE LOCATION OF ALL TRAFFIC CONTROL DEVICES AND LOW-PROFILE BARRICADES WITH THE AIRPORT OPERATIONS.
6. NO OPEN TRENCHES SHALL BE PERMITTED WITHIN THE SAFETY AREAS OF ACTIVE RUNWAYS OR TAXIWAYS.
7. PRIOR TO REOPENING THE RUNWAYS AND TAXIWAYS AFTER CLOSURES, THE SAFETY AREAS SHALL BE:

A. CLEARED AND GRADED AND HAVE NO POTENTIALLY HAZARDOUS RUTS, HUMPS, DEPRESSIONS, OR OTHER SURFACE VARIATIONS.

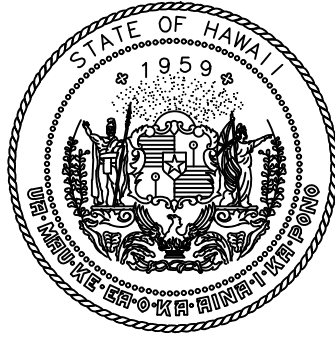
B. NO ABRUPT CHANGES IN GRADE SUCH AS DROPS OR LIPS GREATER THAN 3-INCHES.

C. DRAINED BY GRADING TO PREVENT WATER ACCUMULATION.

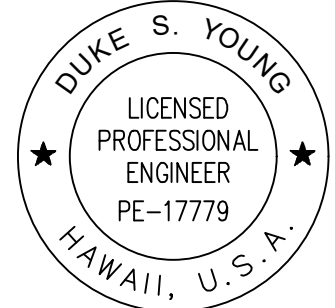
D. CAPABLE, UNDER DRY CONDITIONS, OF SUPPORTING AIRCRAFT RESCUE AND FIRE FIGHTING EQUIPMENT, AND THE OCCASIONAL PASSAGE OF AIRCRAFT WITHOUT CAUSING STRUCTURAL DAMAGE TO THE AIRCRAFT.

E. FREE OF FOREIGN OBJECTS, WHICH CAN DAMAGE AIRCRAFT.
8. AN INSPECTION WILL BE PERFORMED BY THE CONSTRUCTION MANAGER AND AIRPORT OPERATIONS, 30 MINUTES PRIOR TO REOPENING THE RUNWAY/TAXIWAY TO DETERMINE IF THE ABOVE CRITERIA ARE MET.
9. AT THE END OF EACH SHIFT/NIGHT, THE WORK AREA SHALL BE RETURNED TO FULL OPERATIONAL CONDITIONS INCLUDING REMOVING BARRICADES AND RESTORING PAVEMENT MARKINGS, LIGHTING AND SIGNAGE TO ORIGINAL CONDITIONS AND CLARITY. FAILURE TO OPEN THE AIRFIELD ON-TIME OR TO RETURN ALL RUNWAY AND TAXIWAY LIGHTING TO FULL OPERATIONAL CONDITION WILL RESULT IN LIQUIDATED DAMAGES.
10. CONTRACTOR SHALL STOP WORK AND ALLOW AT LEAST 30 MINUTES FOR ELECTRICAL TROUBLESHOOTING PRIOR TO THE REOPENING OF THE RUNWAY/TAXIWAY. THE CONTRACTOR SHALL BE READY TO PROVIDE TEMPORARY JUMPER CABLES NECESSARY TO RE-ENERGIZE THE AIRFIELD ELECTRICAL SYSTEM IN CASE OF COLLAPSED CONDUITS OR OTHER UNFORESEEN ISSUES THAT PREVENT FULL RESTORATION AT THE END OF THE SHIFT. TEMPORARY JUMPER CABLES SHALL BE INSTALLED WITHIN ABOVE GROUND CONDUITS AND ANCHORED FROM JET BLAST.
11. CONTRACTOR SHALL PROVIDE FOD, DUST, AND SMOKE CONTROL AT ALL TIMES. HAUL ROUTES CROSSING ACTIVE AIRCRAFT PAVEMENTS (RUNWAYS, TAXIWAYS AND APRONS) AND PUBLIC ROADS SHALL BE CONTINUOUSLY CLEANED WITH A VACUUM SWEEPER.
12. WASTE MATERIAL OR DIRT STOCKPILES SHALL NOT BE TEMPORARILY STORED WITHIN THE RUNWAY OR TAXIWAY OBJECT FREE AREA. ALL MATERIALS SHALL BE REMOVED FROM THE AIRPORT AND PLACED IN DESIGNATED STOCKPILE LOCATION OR DISPOSED OF AT THE END OF EACH WORK SHIFT.
13. CONSTRUCTION TRAFFIC ON THE AIRPORT IS LIMITED TO HAUL AND ACCESS ROUTES AS SHOWN ON PLANS. HAUL ROUTES AND PAVEMENT, INCLUDING ADJACENT AFFECTED AREAS, SHALL BE KEPT IN VACUUM BROOM CLEAN CONDITION AT ALL TIMES.
14. HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED TO THEIR ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT. THE BEFORE AND AFTER CONDITION OF THE ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED, PHOTOGRAPHED, AND AGREED UPON BY THE CONTRACTOR AND THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ANY DAMAGE TO THE HAUL ROUTES RESULTING FROM CONSTRUCTION ACTIVITY, INCLUDING REPAIR OF STRIPING.

15. CONTRACTOR SHALL PROVIDE SUFFICIENT FLAG PERSON TO ENSURE CONSTRUCTION VEHICLES USING THE CONSTRUCTION HAUL ROAD DO NOT INTERRUPT TRAVEL OF AIRCRAFT. FLAG PERSONS SHOWN ON THESE PLANS ARE THE MINIMUM THE CONTRACTOR MUST PROVIDE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH AIRPORT OPERATIONS IF FIELD CONDITIONS WARRANT ADDITIONAL FLAG PERSONS OR ESCORT VEHICLES ESPECIALLY DURING HEAVY CONSTRUCTION ACTIVITES WITH MATERIAL DELIVERIES AND EXCAVATIONS.
16. THE CONTRACTOR'S LAYDOWN & STORAGE AREA AS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL SUBMIT A LAYOUT PLAN OF THE FINAL SIZE & LOCATION OF STAGING AREAS TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO MOBILIZATION. TEMPORARY FENCING, POWER AND UTILITY LINES, ARE THE RESPONSIBILITY OF THE CONTRACTOR.
17. THE CONTRACTOR SHALL PROVIDE AND APPLY DUST CONTROL AT ALL TIMES, AS REQUIRED, TO ABATE NUISANCE DUST WHICH IS A DIRECT RESULT OF CONSTRUCTION ACTIVITIES ON AND AROUND THE CONSTRUCTION AND STAGING AREAS. CONTRACTOR SHALL CLEAN THE LENSES OF THE ASOS AT THE MIDDLE AND END OF EACH WORK SHIFT.
18. ANY VEHICLE AND EQUIPMENT THAT MUST TRANSIENT THROUGH THE INSTRUMENT LANDING SYSTEM (ILS) CRITICAL AREA OR LOCALIZER (LOC) CRITICAL AREA, SHALL NOT BE PARKED OR STOPPED IN THIS AREA FOR ANY REASON. PROVIDE 72 HOUR NOTIFICATION TO THE AIRPORT PRIOR TO ACCESSING WORK THROUGH THIS AREA. A NOTICE TO AIRMEN (NOTAM) MUST BE FILED FOR WORK IN THE ILS & LOC CRITICAL AREAS. NO EQUIPMENT OR MATERIAL STORAGE/STOCKPILING WILL BE ALLOWED WITHIN THE ILS & LOC CRITICAL AREAS. ANY WORK OR TRAVERSING THROUGH THIS AREA COULD DEGRADE THE ILS SIGNAL AND IMPACT AIRPORT OPERATIONS DURING INSTRUMENT FLIGHT RULES (IFR) CONDITIONS. THE CONTRACTOR MAY BE REQUIRED TO RESCHEDULE WORK DURING INSTRUMENT WEATHER CONDITIONS.
19. CONTRACTOR SHALL REMAIN OUTSIDE OF VOR CRITICAL AREA AT ALL TIMES. ANY VEHICLE AND EQUIPMENT THAT MUST TRANSIENT THROUGH THE VOR CRITICAL AREA, SHALL NOT BE PARKED OR STOPPED IN THIS AREA FOR ANY REASON. PROVIDE 72 HOUR NOTIFICATION TO THE AIRPORT PRIOR TO ACCESSING WORK THROUGH THIS AREA.
20. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN EXCAVATING IN AREAS OF EXISTING UTILITIES. EXISTING UTILITIES SHALL BE LOCATED AND MARKED IN ADVANCE OF EXCAVATION IN ALL AREAS. ANY DAMAGE DONE TO UTILITIES SHALL BE REPAIRED IMMEDIATELY BY THE CONTRACTOR AT HIS EXPENSE. THE LOCATION OF ANY UTILITIES SHOWN ON THE PLANS IS APPROXIMATE ONLY, AND DEPTHS MAY NOT BE KNOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPLACING OR REPAIRING ALL DAMAGE TO UTILITIES AND AIRPORT PROPERTY.
21. FAA FORM 7460-1 SHALL BE FILED BY THE CONTRACTOR A MINIMUM OF 3 MONTHS IN ADVANCE OF CONSTRUCTION FOR THIS PROJECT AND FOR ALL CRANE ACTIVITY ASSOCIATED, AND OBTAIN APPROVAL FROM FAA PRIOR TO WORK BEING PERFORMED. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT A MINIMUM OF 14 DAYS IN ADVANCE FOR ANY REQUIRED NOTAMS AND NAVAID SHUTDOWNS.
22. ONCE A PHASE HAS STARTED, THAT PHASE SHALL BE COMPLETED IN THE TOTAL NUMBER OF CONTINUOUS CALENDAR DAYS AS INDICATED.
23. CONTRACTOR HAUL ROUTE WILL BE USED BY OTHER AIRPORT VEHICLES. THE CONTRACTOR SHALL NOT INTERFERE WITH OTHER VEHICLE TRAFFIC, OBEY SPEED LIMITS, AND SHALL YIELD TO ALL EMERGENCY VEHICLES AND AIRCRAFT. ALL EXISTING GATES SHALL BE UNOBSTRUCTED AND OPERATIONAL AT ALL TIMES.
24. CONTRACTOR SHALL BE RESPONSIBLE TO ESTABLISH NEW HAUL ROUTES OR IMPROVE EXISTING ROADS AS SHOWN ON THE PLANS. OR AS HE/SHE DEEMS NECESSARY IN ORDER TO COMPLETE THE PROJECT SCOPE.
25. CONTRACTOR SHALL COORDINATE WITH THE AIRPORT FOR ANY NEW CONSTRUCTION ACCESS GATES, AND PROVIDE AIRPORT QUALIFIED SECURITY PERSONNEL TO PERFORM REQUIRED CHECKS AND INSPECTIONS TO MAINTAIN AIRPORT SECURITY.
26. LIH ATCT NORMAL OPERATIONAL HOURS ARE 0600 TO 2200.



Airports Division
DEPARTMENT OF TRANSPORTATION
STATE OF HAWAII



Duke Young
DUKE YOUNG, PE
CIVIL ENGINEER

4/30/24
Expiration Date

DSGN.	DRWN.	CHKD.	APPD.
HF	HT	JB	DY

KEY PLAN / NOTES:

NO.	DATE	REVISIONS
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CONSTRUCTION
DOCUMENTS
JULY 2022
DATE

PROJECT TITLE :

RELOCATE
RUNWAY 3-21

AT
LIHUE AIRPORT
LIHUE, KAUAI, HAWAII

PROJECT NO.:

AK1031-14

SHEET TITLE:

GENERAL PHASING
NOTES

DATE :	DWG. NO. <div>G0.31</div>
07/2022	
SHEET :	
22 OF 376 SHEETS	

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