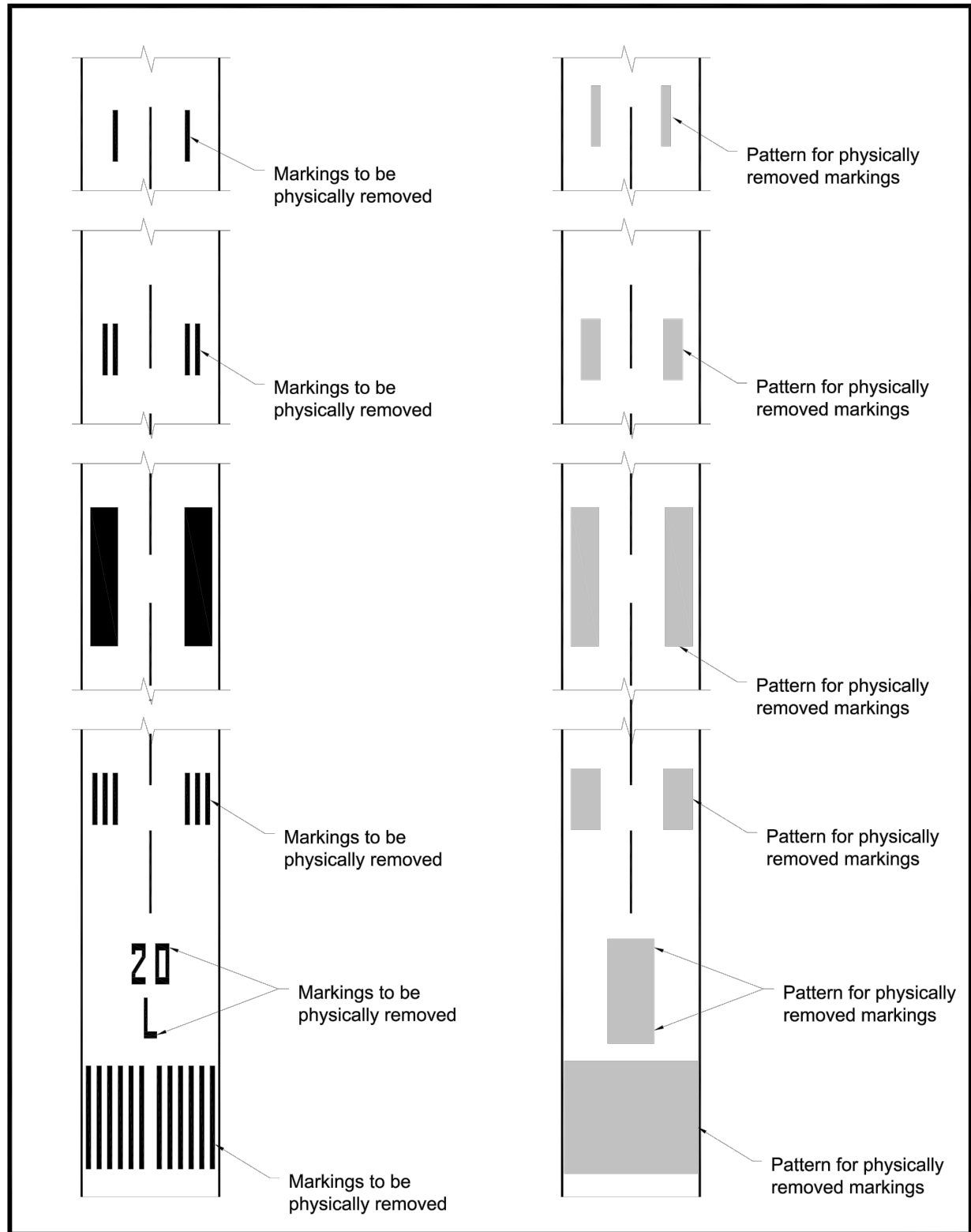


Figure 1-1. Example of Marking Removal Patterns

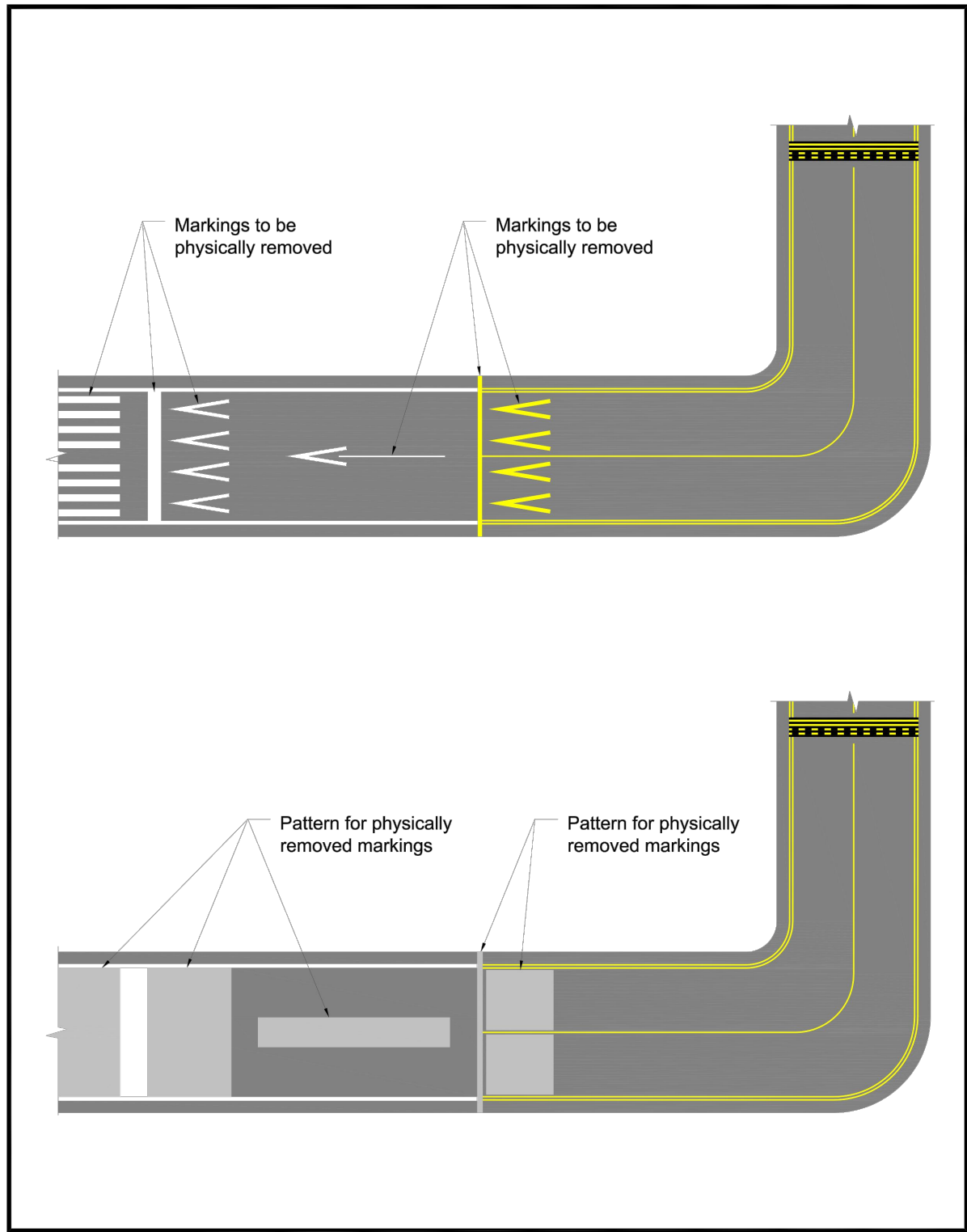


Note: For further details, see paragraph 1.3.6.

AC 150/5340-1M - FIG 1-1
SCALE: N.T.S.

12

Figure 1-3. Example of Marking Removal Patterns

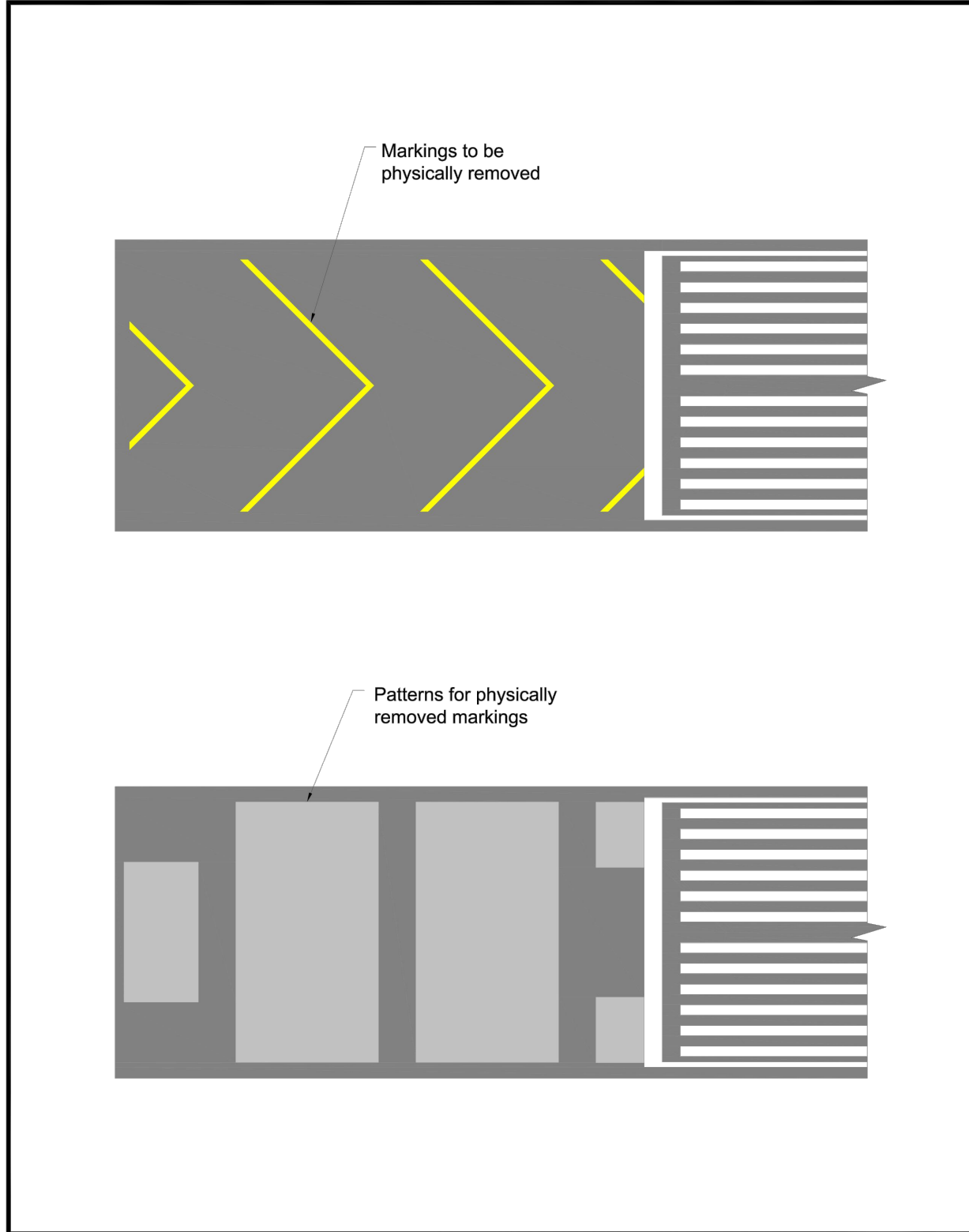


Notes: For further details, see paragraph 1.3.6.
Aligned taxiway construction is prohibited per AC 150/5300-13. Existing locations should be rectified.

AC 150/5340-1M - FIG 1-3
SCALE: N.T.S.

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Figure 1-2. Example of Marking Removal Patterns

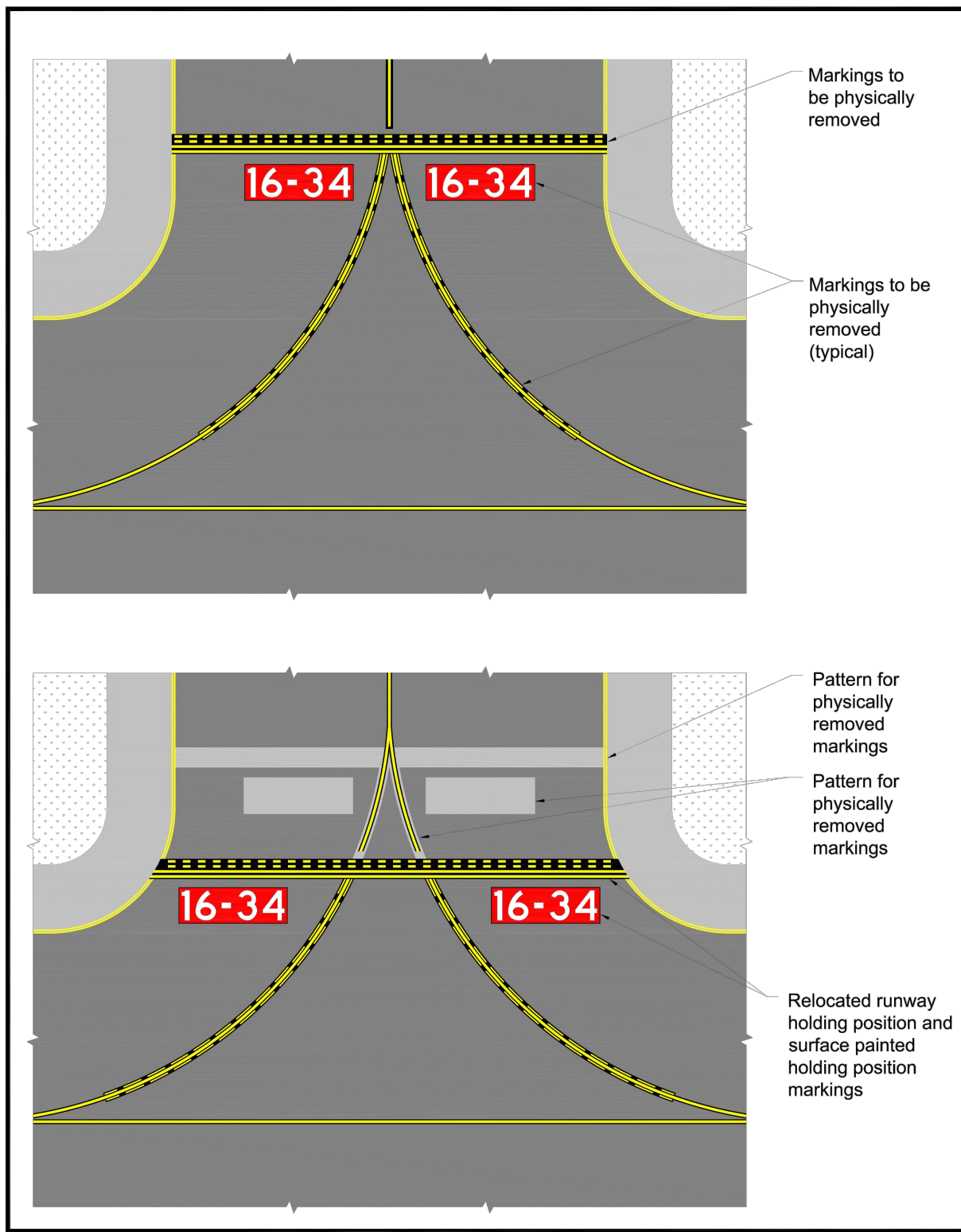


Note: For further details, see paragraph 1.3.6.

AC 150/5340-1M - FIG 1-2
SCALE: N.T.S.

13

Figure 1-4. Example of Marking Removal Patterns



Note: For further details, see paragraph 1.3.6.

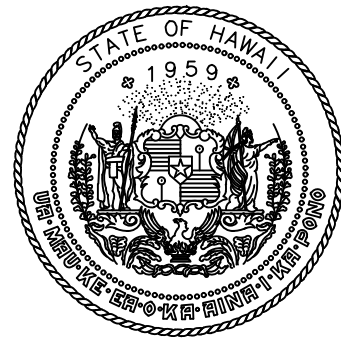
AC 150/5340-1M - FIG 1-4
SCALE: N.T.S.

15

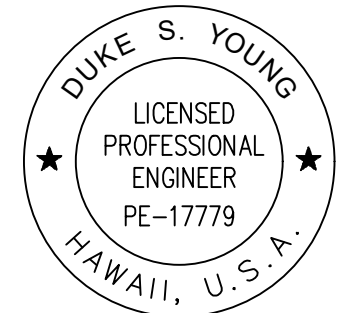
1.3.6

Removal of Markings.

Pavement markings that are no longer needed are not to be painted over but instead are to be physically removed. The FAA does not endorse painting over the old markings because this inadequate practice merely preserves the old marking which, in some cases, has misled pilots and required extra maintenance. Physical removal of markings is achieved by water blasting, shot blasting, sand blasting, chemical removal, or other acceptable means that do not harm the pavement. The physical removal of any old marking(s) must include a predetermined larger size and shape of a removal area that encompasses the old marking(s) and by grouping adjacent markings together into a larger rectangular removal area. The rationale behind this practice is to eliminate the continued visual appearance of the removed marking(s). When a runway end or threshold is being moved, all of the markings that are being removed must be strikingly larger in size, grouped together with adjacent markings, and be rectangular in shape. For example, use a single, larger rectangular removal area to encompass the entire runway designator 7 or 14L and provide a separate large rectangular area comprising all of the runway threshold markings along with a separate large rectangular area for the touchdown zone markings on the same side of the runway centerline. Also, the size and shape for the removal area for (1) Patterns A, B, and C holding position markings and (2) yellow arrow heads having a runway threshold bar or a runway demarcation bar must be much larger than the marking(s) being removed. For example, use a single, larger rectangular removal area to encompass all yellow arrow heads and the adjacent white runway threshold bar marking. See Figure 1-1, Figure 1-2, Figure 1-3, and Figure 1-4 for examples of marking removal patterns.



Airports Division
DEPARTMENT OF TRANSPORTATION
STATE OF HAWAII



Duke Young
DUKE YOUNG, PE
CIVIL ENGINEER

4/30/24
Expiration Date

DSGN.	DRWN.	CHKD.	APPD.
HF	HT	JB	DY

KEY PLAN / NOTES:

NO.	DATE	REVISIONS
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CONSTRUCTION
DOCUMENTS
JULY 2022
DATE

PROJECT TITLE :

RELOCATE
RUNWAY 3-21

AT
LIHUE AIRPORT
LIHUE, KAUAI, HAWAII

PROJECT NO.:

AK1031-14

SHEET TITLE:

DEMOLITION
DETAILS

DATE :	DWG. NO.
07/2022	C2.50
SHEET :	
137 OF 376 SHEETS	