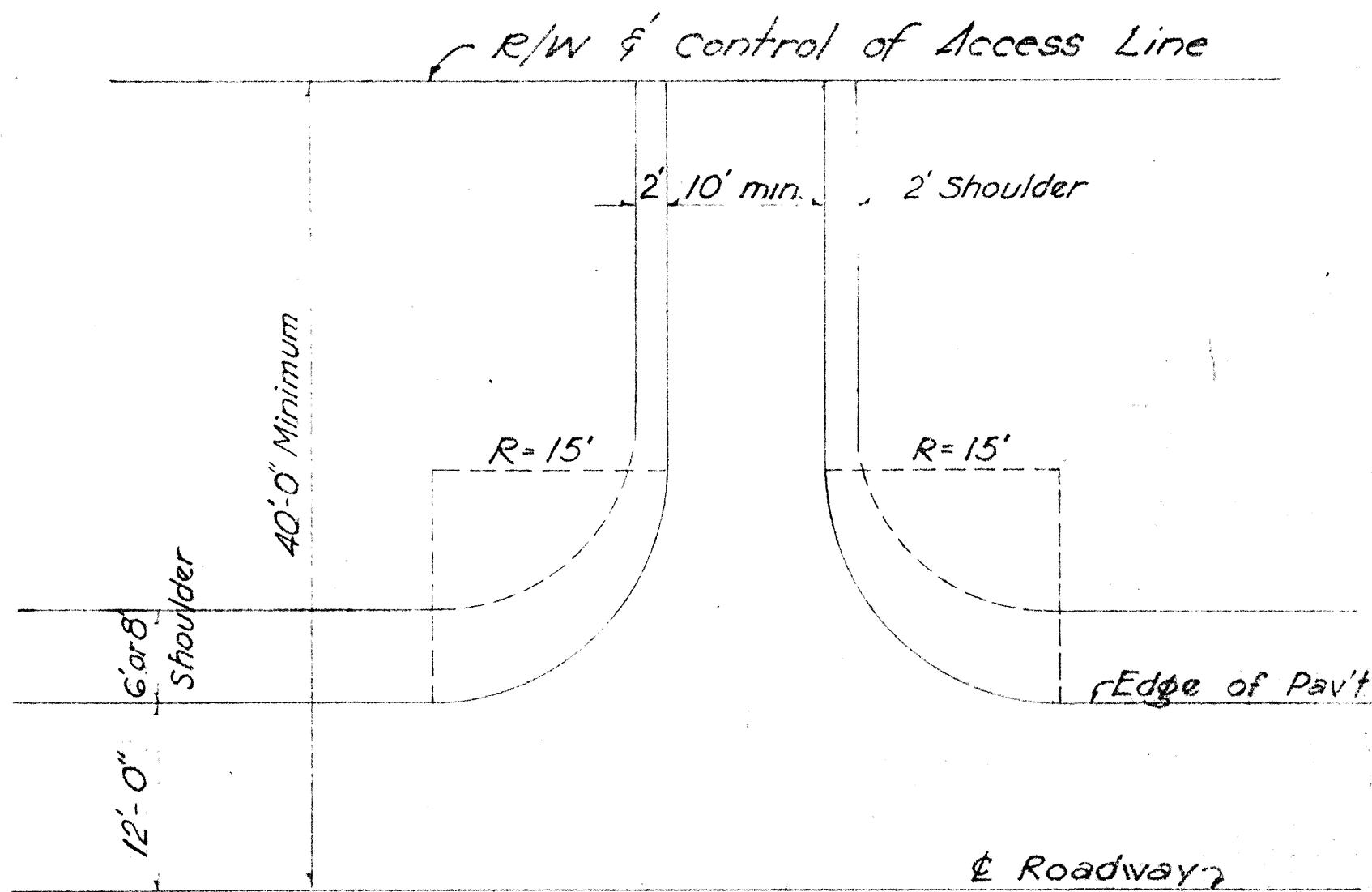
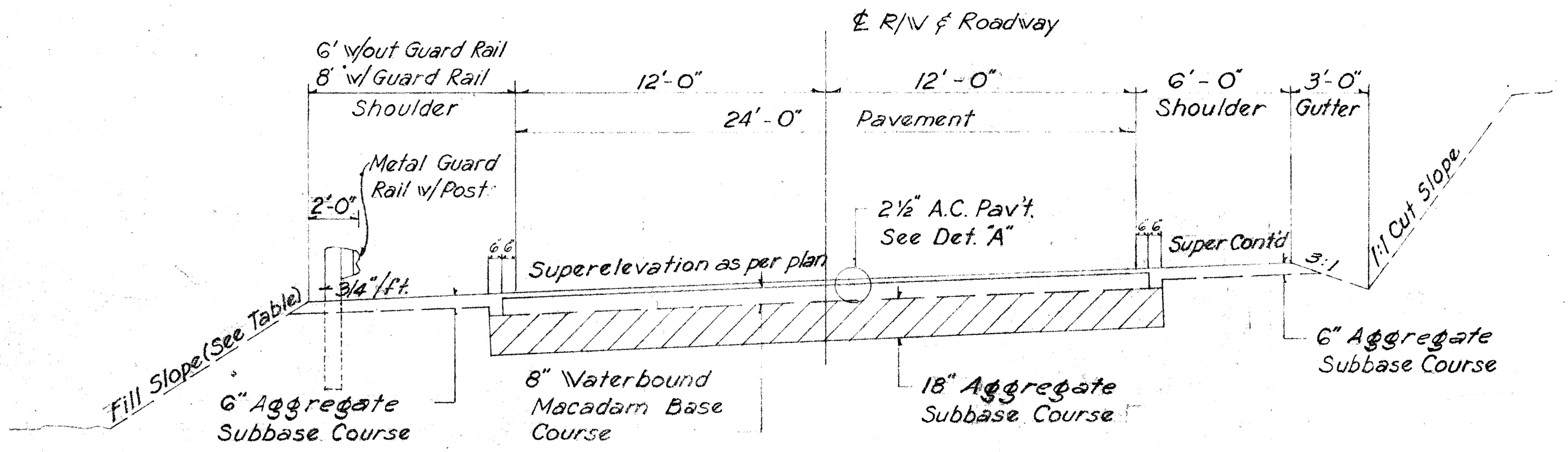


TYPICAL ROAD SECTION ON TANGENT
Scale: 1/4" = 1'-0"



TYPICAL PRIVATE DRIVEWAY PLAN
Scale: 1" = 10'-0"

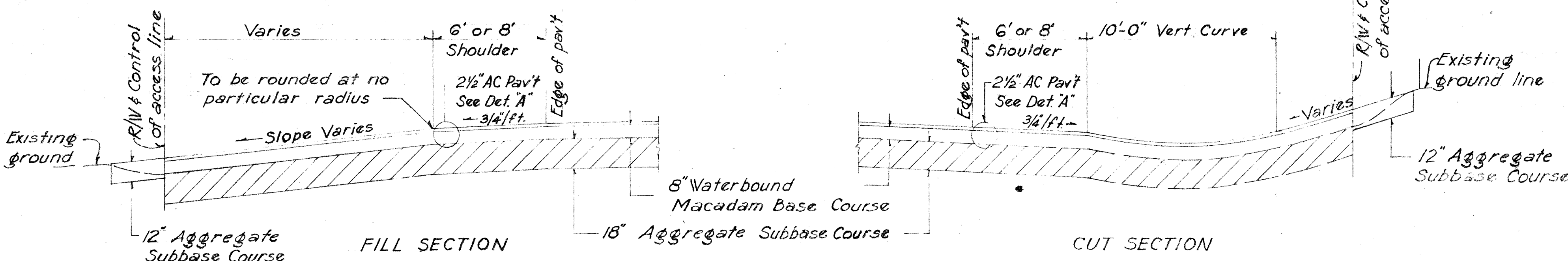


TYPICAL ROAD SECTION ON CURVE
Scale: 1/4" = 1'-0"

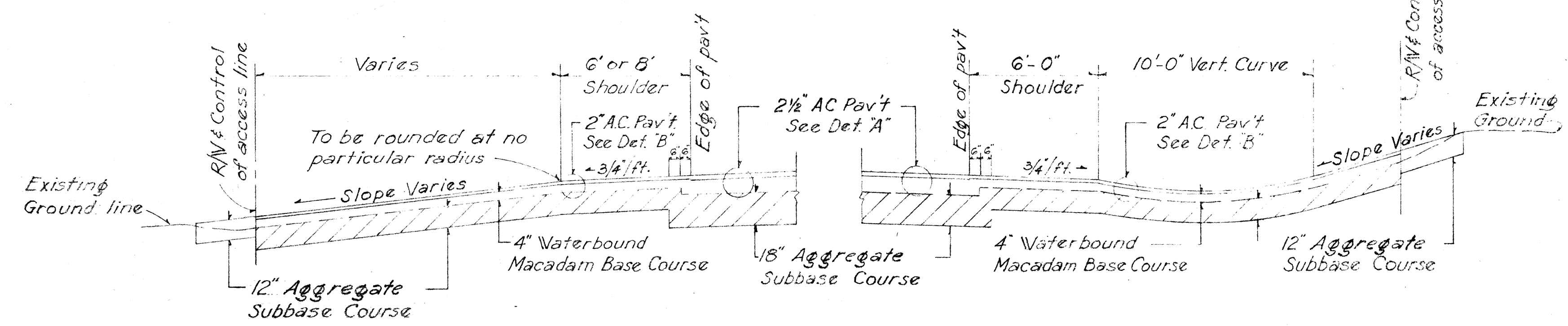
FILL SLOPE TABLE

Height of Fill	Fill Slope
0 - 4'	4 : 1
4 - 10'	3 : 1 To 2 : 1
10 - Over	1 1/2 : 1

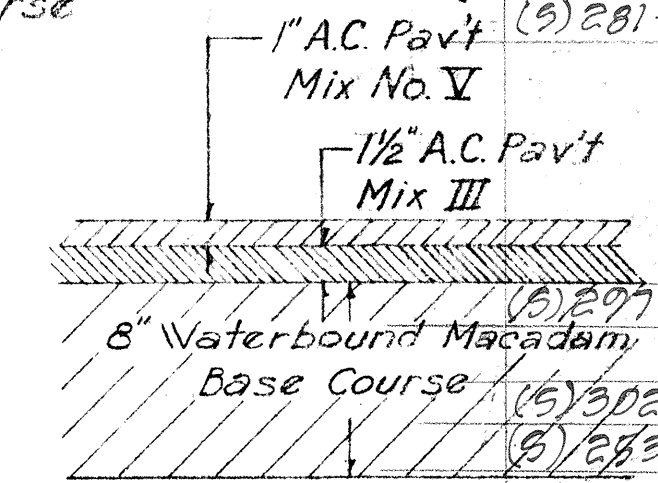
R/W Requirement Supercedes above Table.



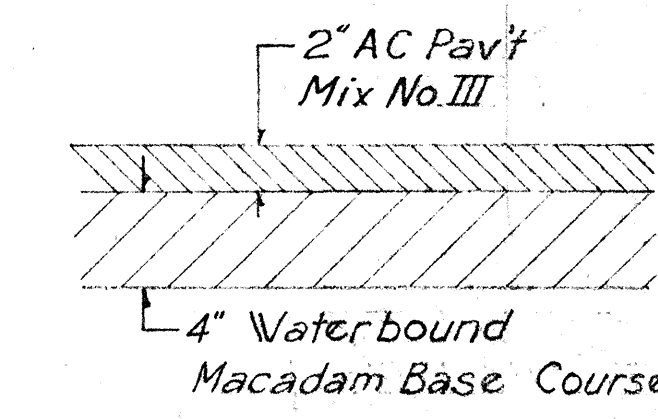
TYPICAL SECTIONS OF SIDEROAD APPROACHES
Scale: 1/4" = 1'-0"



TYPICAL SECTIONS OF PRIVATE DRIVEWAYS
Scale: 1/4" = 1'-0"



DETAIL "A"
Scale: 1/2" = 1'-0"



DETAIL "B"
Scale: 1/2" = 1'-0"

Construct Smooth and Easy Riding Connection to Existing Driveways and Sideroad Approaches

Station	Pav't Area (sq)	B.C. Area (sq)	Exc. (cy)	Emb. (cy)	Subbase Material (Ton)
(S) 184+00 (L)	87	92	37		64
(S) 184+30 (R)	278	270	93		201
(D) 184+30 (L)	42	66	35		36
(D) 184+35 (L)	48	73	37		37
(D) 186+09 (L)	42	66	35		36
(D) 187+90 (L)	67	91	46		46
(D) 187+00 (L)	57	81	27		41
(D) 187+36 (L)	42	66	12		36
(D) 190+02 (L)	42	66	23		36
(D) 190+82 (L)	42	66	47		36
(S) 191+70 (L)	156	163	69		123
(D) 193+00 (L)	42	66	71		36
(D) 193+27 (L)	42	66	71		36
(D) 193+83 (L)	42	66	81		36
(D) 194+50 (L)	48	73	17		39
(D) 195+15 (D)	42	66	22		36
(S) 195+70 (L)	156	163	74		123
(D) 196+53 (L)	42	66	18		36
(S) 197+50 (R)	156	163	71		123
(D) 197+60 (L)	67	91	20		46
(S) 200+50 (L)	156	163	136	Exc.	123
(S) 200+50 (R)	156	163	68	(C.Y.)	123
(S) 206+25 (R)	189	197	1,000	1,000	148
(S) 207+50 (L)	189	197	1,000	1,000	148
(S) 209+50 (R)	211	220	1,000	1,000	166
(S) 215+75 (R)	189	197	1,000	1,000	148
(S) 222+50 (R)	189	197	1,000	1,000	148
(S) 225+25 (L)	189	197	1,000	1,000	148
(S) 233+50 (R)	156	163	1,000	1,000	123
(S) 235+25 (R)	211	220	1,000	1,000	166
(S) 237+25 (L)	189	197	1,000	1,000	148
(S) 237+25 (R)	156	163	1,000	1,000	123
(S) 239+30 (R)	211	220	1,000	1,000	166
(S) 246+50 (R)	156	163	1,000	1,000	123
(S) 247+75 (L)	189	197	1,000	1,000	148
(S) 249+75 (R)	156	163	1,000	1,000	123
(S) 256+00 (L)	156	163	1,000	1,000	123
(S) 256+00 (R)	156	163	1,000	1,000	123
(S) 265+10 (L)	189	197	1,000	1,000	148
(S) 264+00 (R)	156	163	1,000	1,000	123
(S) 272+00 (L)	189	197	1,000	1,000	148
(S) 272+00 (R)	189	197	1,000	1,000	148
(S) 277+50 (R)	189	197	1,000	1,000	148
(S) 281+55 (L)	189	197	1,000	1,000	148
(S) 281+55 (R)	189	197	1,000	1,000	148
(D) 290+70 (L)	42	66	11	34.7	36
(S) 291+21 (L)	156	163	1,000	1,000	123
(S) 294+00 (R)	401	404	734	4,609.0	226
(D) 294+13 (L)	42	66	594	20	36
(D) 294+31 (L)	42	66	594	20	36
(S) 297+70 (L)	156	163	1,000	1,000	123
(S) 297+75 (L)	156	163	1,000	1,000	123
(S) 299+65 (R)	156	163	1,000	1,000	123
(S) 302+15 (L)	156	163	1,000	1,000	123
(S) 303+17 (R)	144	143	1,000	1,000	1008
(S) 303+00 (R)	144	143	1,000	1,000	1008
(S) 303+50 (L)	144	143	1,000	1,000	1008
(D) 307+20 (L)	42	66	11	34.2	36
(D) 307+70 (L)	477	543	543	49.2	19.6
(D) 307+74 (L)	477	543	543	49.2	19.6
(D) 300+40 (L)	477	543	543	49.2	19.6
(D) 307+17 (R)	477	543	543	49.2	19.6
(D) 307+85 (L)	210	210	210	13.6	13.6

(D) Driveway
(S) Sideroad Approaches

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

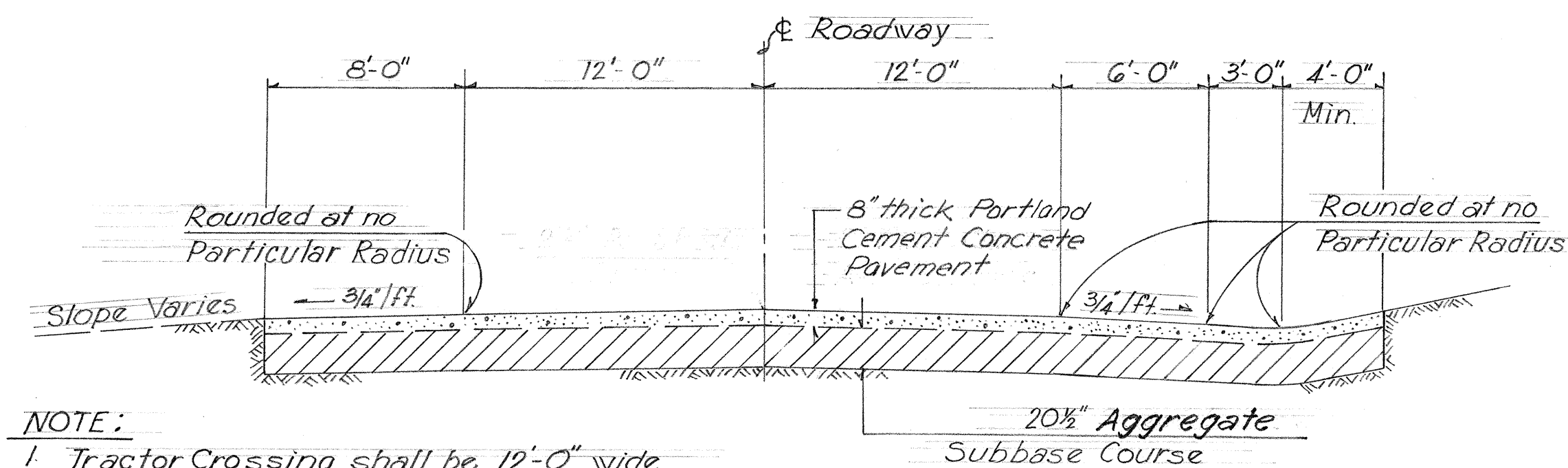
TYPICAL ROAD, SIDEROAD
AND DRIVEWAY SECTIONS

HONOKAA-MAIPIO ROAD Proj. No. 240A-01-67

SHEET No. 1 OF 2 SHEETS

DATE
SURVEY PLATTED BY
DESIGNED BY
CHECKED BY
ORIGINAL PLAN
NOTE BOOK
QUANTITIES BY

STATE	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAW.	1970	3	69



- NOTE:
- Tractor Crossing shall be 12'-0" wide and shall conform to pavement grade, crown and shoulder slope.
 - Tractor Crossing to be reinforced with 6x6x6 Ga. wire mesh reinforcement.

TYPICAL SECTION OF TRACTOR CROSSING
Scale: 3/16" = 1'-0"

CONSTRUCT TRACTOR CROSSING			
Station	Length	Width	Est. Concrete Quantities, C.Y.
191+44	50'	12'	15
209+25	47'	12'	14
225+25	50'	12'	15
236+60	44'	12'	13
243+50	47'	12'	14
265+50	47'	12'	14
281+38	50'	12'	15
301+88	44'	12'	13

CONSTRUCT TRACTOR CROSSING			
Station	Length	Width	Concrete Quantities, C.Y.
209+36	48.2'	12'	14.35
227+12	48.4'	12'	14.41
237+11	48.4'	12'	14.41
249+61	48.3'	12'	14.38
255+86	48.2'	12'	14.36
281+41	48.1'	12'	14.33
301+99	40.0'	12'	11.92

GUARD RAIL SCHEDULE			
Station (Left)	Lineal Feet	Station (Right)	Lineal Feet
197+80 to 198+30	50	197+80 to 197+30	50
201+05 to 206+50	550	201+25 to 206+10	485
210+00 to 211+00	100	209+35 to 211+00	115
215+50 to 221+50	600	216+20 to 219+50	330
227+00 to 234+00	700	227+05 to 231+10	405
239+45 to 241+55	210	239+35 to 241+00	105
243+65 to 244+40	75	243+40 to 244+40	100
256+65 to 260+50	385	273+00 to 276+40	340
262+60 to 264+60	200	282+05 to 284+00	195
266+35 to 269+60	265	285+80 to 288+05	225
272+35 to 276+55	360	300+55 to 301+80	125
280+00 to 281+15	115	198+90 to 199+65	75
282+20 to 284+00	180	245+60 to 246+35	75
285+00 to 288+50	350		
300+55 to 301+80	125		
198+90 to 199+90	100		
245+60 to 249+10	350		

GUARD RAIL SCHEDULE			
Station (Left)	Lineal Feet	Station (Right)	Lineal Feet
201+05 to 206+50	491.4	201+25 to 206+10	485.6
210+00 to 211+00	104.2	209+35 to 211+00	129.1
215+50 to 221+50	603.0	216+20 to 219+30	341.6
222+00 to 224+15	216.0	228+00 to 231+10	393.7
227+20 to 228+45	128.7	240+15 to 241+30	141.2
229+15 to 234+00	472.5	243+75 to 244+40	66.2
234+65 to 236+90	241.2	245+60 to 246+10	53.7
239+45 to 241+55	216.2	254+60 to 255+75	116.5
243+90 to 244+40	53.7	267+30 to 267+75	41.5
245+60 to 249+50	391.0	273+00 to 276+40	166.4
253+95 to 255+75	180.2	282+05 to 284+00	191.2
256+65 to 260+50	390.8	285+80 to 288+05	235.0
262+40 to 264+60	215.9	300+55 to 301+80	128.8
265+60 to 271+85	628.9		
272+95 to 281+15	865.5		
282+20 to 284+00	241.2		
285+00 to 288+50	341.1		
300+55 to 301+80	128.7		

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION
TYPICAL SECTION OF TRACTOR
CROSSING AND
GUARD RAIL SCHEDULE
HONOKAA-WAIPIO ROAD Proj. No. 240A-01-67