

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	19H-01-91M	1991	5	13

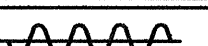
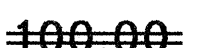
BEGINNING OF PROJECT
Sta. ~~35+86~~
35+80

TRUE NORTH
SCALE: 1" = 40'

CURVE DATA ①
 $\Delta = 62^\circ 29'$
 $\Delta_c = 27^\circ 29'$
 $D = 14'$
 $\theta_s = 17^\circ 30'$
 $R = 409.26'$
 $L_c = 196.31'$
 $L_s = 250.00'$
 $T_s = 376.72'$

CURVE DATA ②
 $\Delta = 44^\circ 48'$
 $\Delta_c = 9^\circ 48'$
 $D = 14'$
 $\theta_s = 17^\circ 30'$
 $R = 409.26'$
 $L_c = 70.00'$
 $L_s = 250.00'$
 $T_s = 295.91'$

CURVE DATA ③
 $\Delta = 110^\circ 34' 07''$
 $\Delta_c = 93^\circ 04' 07''$
 $D = 14'$
 $\theta_s = 17^\circ 30'$
 $R = 409.26'$
 $L_c = 664.78'$
 $L_s = 250.00'$

LEGEND FOR AS-BUILT POSTINGS
 squiggly line for as-built deletion
 double line for as-built deletion
Roadway text for as-built posting

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION
ROADWAY PLANS
HAWAII BELT ROAD RESURFACING
LAUPAHOEHOE GULCH
PROJECT NO. 19H-01-91M
North Hilo, Hawaii
Scale: 1" = 40' Date: Sept. 1990
SHEET No. 1 OF 3 SHEETS

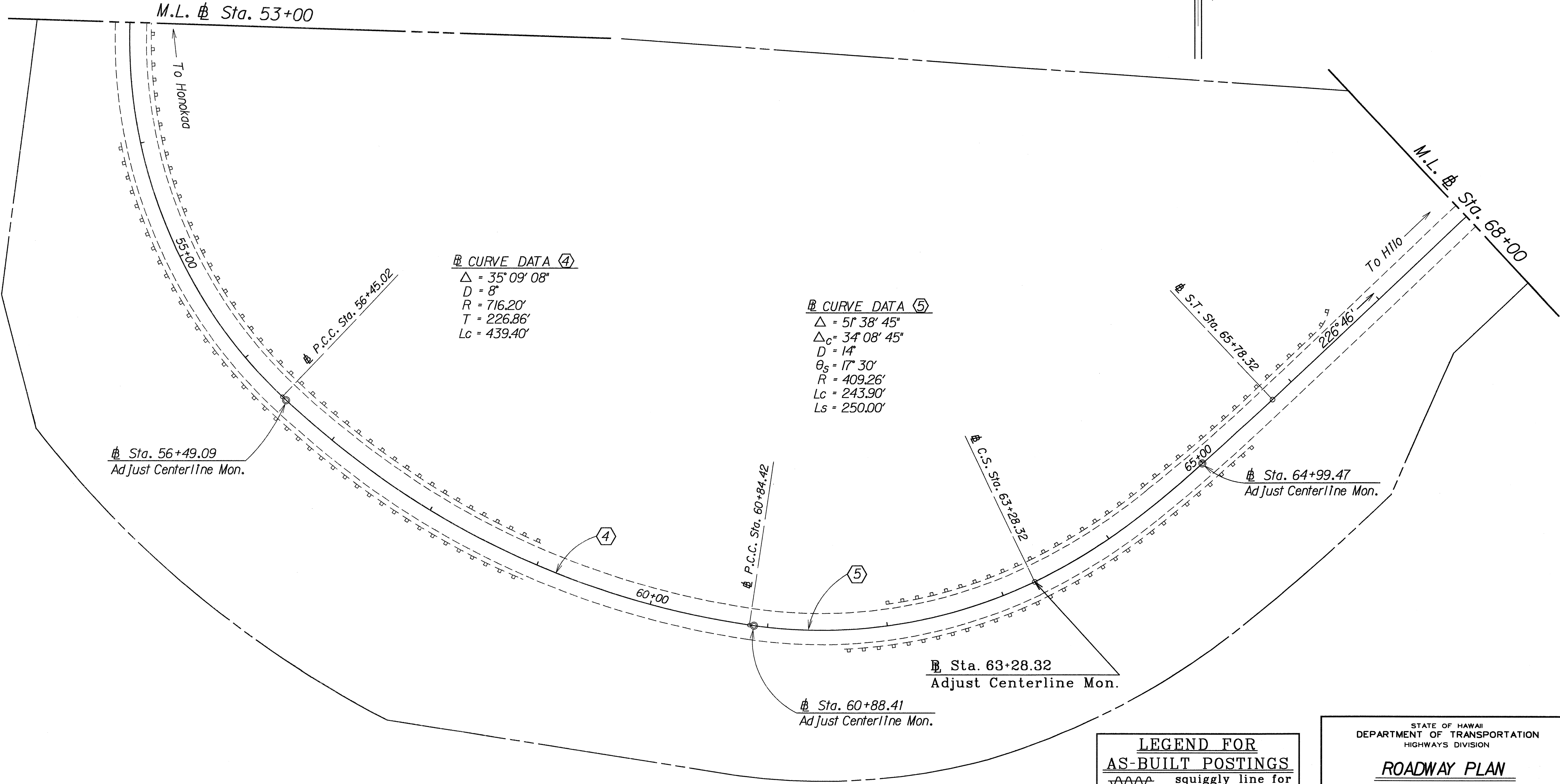
AS-BUILT

5

ORIGINAL PLAN	DATE
DRAWN BY	
CHECKED BY	
NOTED BY	
QUANTITIES BY	
CHECKED BY	
N.	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	19H-01-91M	1991	6	13

TRUE NORTH
SCALE: 1" = 40'



ORIGINAL PLAN	SURVEY PLOTTED BY	DATE
TRACED BY		
DESIGNED BY		
CHECKED BY		
DATE		

LEGEND FOR AS-BUILT POSTINGS	
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	double line for as-built deletion
Roadway text	text for as-built posting

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

ROADWAY PLAN
HAWAII BELT ROAD RESURFACING
LAUPAHOEHOE GULCH
PROJECT NO. 19H-01-91M
North Hilo, Hawaii

Scale: 1" = 40'
Date: Sept. 1990

SHEET No. 2 OF 3 SHEETS

AS-BUILT

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	19H-01-91M	1991	7	13

TRUE NORTH
SCALE: 1" = 40'

6 CURVE DATA 6

$\Delta = 90^{\circ} 20'$
 $\Delta_c = 57^{\circ} 50'$
 $D = 13'$
 $\theta_s = 16^{\circ} 15'$
 $R = 440.74'$
 $L_c = 444.87'$
 $L_s = 250.00'$
 $T_s = 573.91'$

Sta. 72+28.13
Adjust Centerline Mon.

Sta. 79+54.41
Adjust Centerline Mon.

7 CURVE DATA 7

$\Delta = 36^{\circ} 14' 40''$
 $\Delta_c = 26^{\circ} 14' 40''$
 $D = 4'$
 $\theta_s = 5^{\circ} 00'$
 $R = 1432.39'$
 $L_c = 656.11'$
 $L_s = 250.00'$
 $T_s = 594.34'$

LEGEND FOR AS-BUILT POSTINGS

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 === double line for as-built deletion  
 Roadway text for as-built posting

STATE OF HAWAII  
DEPARTMENT OF TRANSPORTATION  
HIGHWAYS DIVISION

ROADWAY PLANS

HAWAII BELT ROAD RESURFACING  
LAUPAHOEHOE GULCH  
PROJECT NO. 19H-01-91M  
North Hilo, Hawaii

Scale: 1" = 40' Date: Sept. 1990

SHEET No. 3 OF 3 SHEETS

AS-BUILT

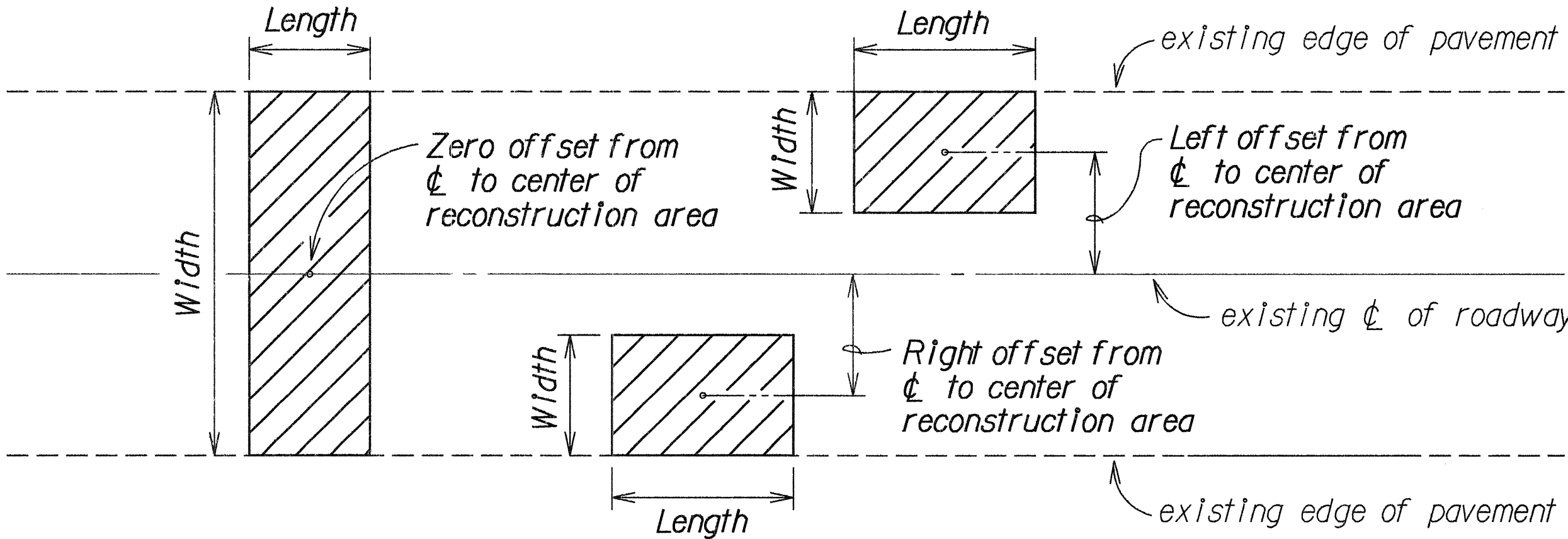


RECONSTRUCTION AREA SCHEDULE

| STATION          |                  | LENGTH         | WIDTH                | OFFSET FROM $\phi$    | APPROX. AREA      |
|------------------|------------------|----------------|----------------------|-----------------------|-------------------|
| FROM             | TO               | (Lin. Ft.)     | (Ft.)                | (Ft.)                 | (Sq. Ft.)         |
| <del>35+80</del> | <del>36+21</del> | <del>41</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>492</del>    |
| <del>35+85</del> | <del>36+10</del> | <del>25</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>300</del>    |
| <del>36+21</del> | <del>36+50</del> | <del>29</del>  | <del>6.3</del>       | <del>9 RT.</del>      | <del>183</del>    |
| <del>37+00</del> | <del>37+25</del> | <del>25</del>  | <del>6</del>         | <del>3 RT.</del>      | <del>150</del>    |
| <del>38+50</del> | <del>37+55</del> | <del>105</del> | <del>6.3</del>       | <del>10 RT.</del>     | <del>662</del>    |
| <del>37+55</del> | <del>38+00</del> | <del>45</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>540</del>    |
| <del>37+55</del> | <del>39+55</del> | <del>200</del> | <del>12</del>        | <del>7 RT.</del>      | <del>2400</del>   |
| <del>39+30</del> | <del>39+80</del> | <del>50</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>600</del>    |
| <del>39+65</del> | <del>39+92</del> | <del>27</del>  | <del>6.3</del>       | <del>10 LT.</del>     | <del>233</del>    |
| <del>39+80</del> | <del>40+20</del> | <del>40</del>  | <del>12</del>        | <del>6 LT.</del>      | <del>480</del>    |
| <del>39+92</del> | <del>41+33</del> | <del>141</del> | <del>12</del>        | <del>7 LT.</del>      | <del>1692</del>   |
| <del>40+40</del> | <del>41+00</del> | <del>60</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>720</del>    |
| <del>41+33</del> | <del>42+06</del> | <del>73</del>  | <del>6.3</del>       | <del>4 LT.</del>      | <del>460</del>    |
| <del>41+00</del> | <del>42+00</del> | <del>100</del> | <del>24</del>        | <del>12 LT.</del>     | <del>2,400</del>  |
| <del>42+06</del> | <del>42+40</del> | <del>34</del>  | <del>6.3</del>       | <del>10 RT.</del>     | <del>214</del>    |
| <del>42+00</del> | <del>42+10</del> | <del>10</del>  | <del>12</del>        | <del>16 LT.</del>     | <del>840</del>    |
| <del>42+40</del> | <del>43+15</del> | <del>75</del>  | <del>12</del>        | <del>7 RT.</del>      | <del>900</del>    |
| <del>45+05</del> | <del>45+55</del> | <del>50</del>  | <del>12</del>        | <del>6 LT.</del>      | <del>600</del>    |
| <del>43+37</del> | <del>43+55</del> | <del>18</del>  | <del>12</del>        | <del>7 RT.</del>      | <del>216</del>    |
| <del>45+40</del> | <del>53+90</del> | <del>850</del> | <del>12</del>        | <del>6 RT.</del>      | <del>10,200</del> |
| <del>44+65</del> | <del>47+00</del> | <del>235</del> | <del>12 (ave.)</del> | <del>7 RT.</del>      | <del>2,820</del>  |
| <del>45+90</del> | <del>47+00</del> | <del>110</del> | <del>18 (ave.)</del> | <del>Varies Lt.</del> | <del>1,980</del>  |
| <del>47+00</del> | <del>47+56</del> | <del>56</del>  | <del>12</del>        | <del>6 RT.</del>      | <del>672</del>    |
| <del>47+00</del> | <del>48+00</del> | <del>100</del> | <del>12</del>        | <del>6 LT.</del>      | <del>1,200</del>  |
| <del>36+45</del> | <del>36+75</del> | <del>30</del>  | <del>613</del>       | <del>13 LT.</del>     | <del>189</del>    |
| <del>50+20</del> | <del>51+10</del> | <del>90</del>  | <del>12</del>        | <del>6 LT.</del>      | <del>1,080</del>  |
| <del>36+75</del> | <del>37+43</del> | <del>68</del>  | <del>3.8</del>       | <del>2 LT.</del>      | <del>258</del>    |
| <del>51+45</del> | <del>52+25</del> | <del>80</del>  | <del>12</del>        | <del>6 LT.</del>      | <del>960</del>    |
| <del>36+75</del> | <del>37+43</del> | <del>68</del>  | <del>2.5</del>       | <del>1 RT.</del>      | <del>170</del>    |
| <del>54+75</del> | <del>56+80</del> | <del>205</del> | <del>12</del>        | <del>6 LT.</del>      | <del>2,460</del>  |
| <del>37+79</del> | <del>38+66</del> | <del>87</del>  | <del>18.3</del>      | <del>13 LT.</del>     | <del>1,592</del>  |
| <del>55+45</del> | <del>56+80</del> | <del>135</del> | <del>12</del>        | <del>6 RT.</del>      | <del>1,620</del>  |
| <del>38+66</del> | <del>39+61</del> | <del>95</del>  | <del>6.5</del>       | <del>3 LT.</del>      | <del>613</del>    |
| <del>57+30</del> | <del>58+05</del> | <del>75</del>  | <del>12</del>        | <del>6 LT.</del>      | <del>900</del>    |
| <del>39+61</del> | <del>39+80</del> | <del>19</del>  | <del>6.3</del>       | <del>9 LT.</del>      | <del>120</del>    |
| <del>59+70</del> | <del>61+40</del> | <del>170</del> | <del>12</del>        | <del>6 LT.</del>      | <del>2,040</del>  |
| <del>39+80</del> | <del>40+22</del> | <del>42</del>  | <del>12</del>        | <del>12 LT.</del>     | <del>504</del>    |
| <del>61+40</del> | <del>63+00</del> | <del>160</del> | <del>12</del>        | <del>6 RT.</del>      | <del>1,920</del>  |

RECONSTRUCTION AREA SCHEDULE

| STATION |       | LENGTH     | WIDTH | OFFSET FROM $\phi$ | APPROX. AREA |
|---------|-------|------------|-------|--------------------|--------------|
| FROM    | TO    | (Lin. Ft.) | (Ft.) | (Ft.)              | (Sq. Ft.)    |
| 64+00   | 65+50 | 150        | 6.3   | 9 RT.              | 945          |
| 66+29   | 67+38 | 109        | 12    | 6 RT.              | 1,308        |
| 70+91   | 71+13 | 22         | 6.3   | 3 RT.              | 139          |
| 70+56   | 70+99 | 43         | 6.3   | 21 RT.             | 271          |
| 66+35   | 68+20 | 185        | 6.3   | 3 LT.              | 1,166        |
| 68+20   | 68+90 | 70         | 12    | 6 LT.              | 840          |
| 68+90   | 69+85 | 95         | 6.3   | 9 LT.              | 599          |
| 70+15   | 70+53 | 38         | 6.3   | 3 LT.              | 239          |
| 70+34   | 70+91 | 57         | 6.3   | 10 LT.             | 359          |
| 70+91   | 71+27 | 36         | 12    | 6 LT.              | 432          |
| 71+60   | 72+45 | 85         | 6.3   | 9 LT.              | 536          |
| 72+45   | 72+90 | 45         | 12    | 6 LT.              | 540          |
| 73+25   | 73+65 | 40         | 12    | 6 LT.              | 480          |
| 73+65   | 74+60 | 95         | 6.3   | 9 LT.              | 599          |
| 74+88   | 75+31 | 43         | 6.3   | 8 LT.              | 271          |
| 75+53   | 78+00 | 247        | 12    | 6 LT.              | 2964         |
| 78+29   | 79+00 | 71         | 12    | 6 LT.              | 852          |
| 47+56   | 52+88 | 532        | 12    | 6 RT.              | 6,384        |
| 52+88   | 54+27 | 139        | 6.3   | 3 RT.              | 876          |
| TOTAL   |       |            |       |                    | 59,025       |



PAVEMENT RECONSTRUCTION - TYPICAL PLAN

Not to Scale

LEGEND FOR  
AS-BUILT POSTINGS

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Roadway text for as-built posting

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

RECONSTRUCTION AREA
SCHEDULE & TYPICAL PLAN

HAWAII BELT ROAD RESURFACING
LAUPAHOEHOE GULCH
PROJECT NO. 19H-01-91M

Date: Sept., 1990

SHEET No. 1 OF 1 SHEETS