

**STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION**

**ADDENDUM NO. 4
for
HAWAII BELT ROAD
KAWAILII STREAM BRIDGE REPLACEMENT
FEDERAL AID PROJECT NO. ER-15(3)**

The following amendments shall be made to the Bid Documents:

A. REQUEST FOR INFORMATION

Disregard responses to RFIs Pre-Bid – 03, Pre-Bid – 05, and Pre-Bid – 09 as included in Addendum No. 3. The responses should read as follows:

Pre-Bid – 03:

Construction sequence on Sheet 80 #2 calls for excavation behind the existing bridge before Sheet 80 #3 which is to demolish the remaining existing retaining walls and construction of the diversion channel shown on the BMP Plan Sheets 30 thru 34. Sequencing would appear to be more appropriate if the two 48-inch HDPE culverts were installed first, the berms diversion channel next, followed by demolition of the remaining retaining walls and existing bridge to minimize the amount of erosion and sediment from entering Kawailii Stream if there should be water flowing.

Construction sequence is shown on Sheet 80. However, as noted on Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Adjusted construction sequences will only be evaluated following award of the contract.

Pre-Bid – 05:

Construction sequence on Sheet 80 #2 calls for excavation in back of the existing bridge not to exceed 2-foot difference in depth. Excavation is to be keyed and benched at a 4'-0" maximum rise and 6'0" maximum run. Special Provision 205.3(a)(1) requires calculations and working drawings for bracing of slopes be stamped by registered professional civil and structural engineers. What is the intent of requiring excavation, as noted above, for a structure to be demolished? Given compliance to Special Provisions 205.3(1)(1), would it be acceptable to excavate for the "keyed" structural backfill after Stage 9 (pouring of deck topping, end beam, and edge beam) after Stage 9 with installation of the fabric reinforced aggregate base course backfill being done after construction of the wingwalls and retaining walls?

Addendum No. 4
2/22/10

Construction sequence is shown on Sheet 80. However, as noted on Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Adjusted construction sequences will only be evaluated following award of the contract.

Pre-Bid – 09:

Plan sheet page 80 (S7.1) implies a very strict sequencing to the project that limits crane access to critical areas of the structure. This limitation will force the need for extensive temporary constructions to provide suitable close access to the structure. Is the depicted sequencing mandatory or is it a suggested sequencing? Can the contractor assume his own means and methods?

Construction sequence is shown on Sheet 80. However, as noted on Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Adjusted construction sequences will only be evaluated following award of the contract.

New RFIs:

Pre-Bid – 14:

How deep should the 3' wide GRP be on the upstream and downstream side of the invert slab?

The cutoff wall for the invert slab is 5' deep, so the GRP should also be 5' deep.

Pre-Bid – 15:

What should the pavement section be like along the makai shoulder from station 53+50 to 57+20? The typical section calls out for HMA Mix IV over the unpaved shoulder, but some depths shown on the cross sections are up to 24".

Since the full pavement section only calls out 10" of HMA, the maximum depth of HMA Mix IV should be 10" also. Any additional fill required below shall be Aggregate Subbase.

Pre-Bid – 16:

Are the shrinkage reducing admixture and corrosion inhibitor requirements (water-cement ratio, bleeding, etc) as noted in specification section 601 for the SBD also the same requirements for the 4000 and 5000 PSI concrete mixes?

For the information requested, see Sheet 45 (Bridge General Notes).

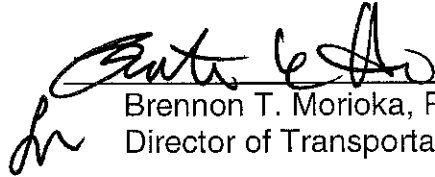
Addendum No. 4
2/22/10

Pre-Bid – 17:

The longitudinal section on plan sheet 51 calls for an additional 2'-0" of 12" #67 Rock beyond the 20' or 10' excavation edge. Is the intent to form the edge of Class D concrete?

Forming edge of Class D Concrete is not required.

Please acknowledge receipt of this Addendum No. 4 by recording the date of its receipt in the space provided on page P-4 of the Proposal.


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