

**STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION**

**ADDENDUM NO. 3
for
HAWAII BELT ROAD
KAWAILII STREAM BRIDGE REPLACEMENT
FEDERAL AID PROJECT NO. ER-15(3)**

The following amendments shall be made to the Bid Documents:

A. NOTICE TO BIDDERS

Prospective bidders are hereby notified that receiving of sealed proposals scheduled for February 18, 2010 will be postponed and rescheduled for 2:00 P.M., March 4, 2010. The attached NOTICE TO BIDDERS shall be incorporated and made a part of the NOTICE TO BIDDERS.

B. SPECIFICATIONS

Instructions for Completing the Final Report of DBE Participation dated 05/07/04 shall be removed from the Bid Documents. No instructions shall be provided for DBE Participation Report dated 08/09.

C. REQUEST FOR INFORMATION

Disregard responses to RFIs Pre-Bid – 03 and Pre-Bid – 05 as included in Addendum No. 1. The responses should read as follows:

Pre-Bid – 03:

Construction sequence on Sheet 80 #2 calls for excavation behind the existing bridge before Sheet 80 #3 which is to demolish the remaining existing retaining walls and construction of the diversion channel shown on the BMP Plan Sheets 30 thru 34. Sequencing would appear to be more appropriate if the two 48-inch HDPE culverts were installed first, the berms diversion channel next, followed by demolition of the remaining retaining walls and existing bridge to minimize the amount of erosion and sediment from entering Kawailii Stream if there should be water flowing.

Construction sequence as shown on Sheet 80 is proposed. However, as noted on the Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Alternative

construction sequences will only be evaluated following award of the contract.

Pre-Bid – 05:

Construction sequence on Sheet 80 #2 calls for excavation in back of the existing bridge not to exceed 2-feet difference in depth. Excavation is to be keyed and benched at a 4'-0" maximum rise and 6'0" maximum run. Special Provision 205.3(a)(1) requires calculations and working drawings for bracing of slopes be stamped by registered professional civil and structural engineers. What is the intent of requiring excavation, as noted above, for a structure to be demolished? Given compliance to Special Provisions 205.3(1)(1), would it be acceptable to excavate for the "keyed" structural backfill after Stage 9 (pouring of deck topping, end beam, and edge beam) after Stage 9 with installation of the fabric reinforced aggregate base course backfill being done after construction of the wingwalls and retaining walls?

Construction sequence as shown on Sheet 80 is proposed. However, as noted on the Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Alternative construction sequences will only be evaluated following award of the contract.

New RFIs:

Pre-Bid – 09:

Plan sheet page 80 (S7.1) implies a very strict sequencing to the project that limits crane access to critical areas of the structure. This limitation will force the need for extensive temporary constructions to provide suitable close access to the structure. Is the depicted sequencing mandatory or is it a suggested sequencing? Can the contractor assume his own means and methods?

Construction sequence as shown on Sheet 80 is proposed. However, as noted on the Sheet 80, construction sequence may be adjusted with the approvals of the Engineer and Department of Health. Changes shall not increase cost nor increase duration of project. Alternative construction sequences will only be evaluated following award of the contract.

Pre-Bid – 10:

Plan sheet page 6 (2 of 2) note E. implies there is a significant potential for NPDES permitting, Water Quality Certification, Stream Channel Alteration Permitting, Section 404 Army Corps of Engineering Permitting, etc. The listed permits represent a significant pursuit (especially for a contractor) and the impact to the project schedule can be equally significant. Can you please

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provide more clarification with regard to these permits and any other such permitting that is expected. Has any jurisdictional issues with regard to Stream/Bridge modifications been pursued and resolved? Or is the pursuit of this process being asked of the successful contractor?

All anticipated permits for the current design were obtained with the exception of the NPDES Permit for Hydrotesting Waters, which the contractor usually obtains.

Pre-Bid – 11:

Can a temporary 6" waterline be suspended from the temporary bridge until the new 6" waterline is installed?

The construction method for the 6" waterline work will be determined by the Contractor. However, the existing waterline is 2.5".

Pre-Bid – 12:

Can the aerial utility lines suspended Mauka of the existing bridge be relocated and temporarily suspended from the transmission poles Makai of the existing bridge?

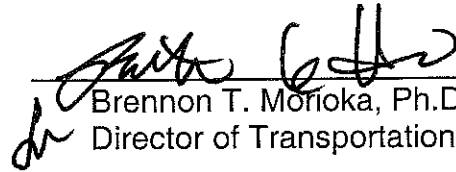
No plans or provisions have been included in the bid documents for the temporary relocation of overhead utilities. In addition, no arrangements been made with the various utility companies for the temporary or permanent relocation of the overhead utilities. Any proposed temporary relocation of overhead utilities shall be reviewed by the Engineer after award of the project. Following acceptance by the Engineer, negotiations/arrangements with the utility companies to proceed with the proposed action shall be the sole responsibility of the contractor. No contract time extension will be granted for coordination with the utility companies or for work associated with the overhead utility relocation.

Pre-Bid – 13:

Please confirm the 450 CY proposal quantity for Item #203.0100. The full pavement area is approximately 11,052 SF x 20" D. (22" minus 2" being cold planed) = 18,420 CF / 27 = 682 CY.

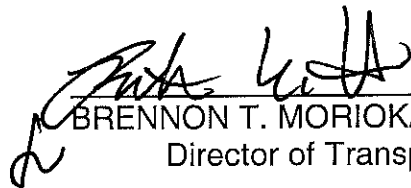
The quantity shown for Item No. 203.0100 in the Proposal Schedule is correct. Please refer to Plan Sheet 7, Note 3 and Plan Sheets 38 to 42 (Boring Logs) for the depth of the cold planing work.

Please acknowledge receipt of this Addendum No. 3 by recording the date of its receipt in the space provided on page P-4 of the Proposal.


Brennon T. Morioka, Ph.D., P.E.
Director of Transportation

NOTICE TO BIDDERS

The receiving of sealed proposals for **HAWAII BELT ROAD, KAWAILII STREAM BRIDGE REPLACEMENT, FEDERAL-AID PROJECT NO. ER-15(3), DISTRICT OF HAMAKUA, ISLAND OF HAWAII**, at the Contracts Office, Department of Transportation, 869 Punchbowl Street, Honolulu, Hawaii 96813, and the Office of the District Engineer – Hawaii, 50 Makaala Street, Hilo, Hawaii 96720, scheduled for 2:00 P.M., February 18, 2010, is hereby **POSTPONED UNTIL 2:00 P.M., March 4, 2010**, at which time and place they will be publicly opened and read.



BRENNON T. MORIOKA, Ph.D., P.E.
Director of Transportation

Internet Posting: February 16, 2010