

TYPICAL SECTION A (Paved Shoulder)

Scale: 3/8"=1'-0"

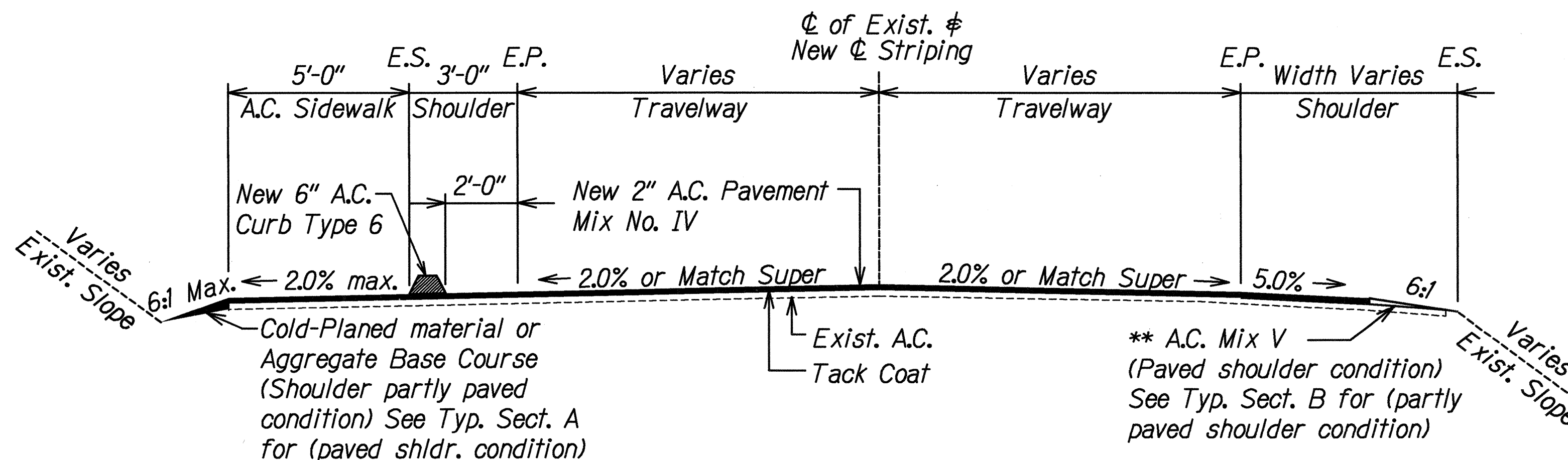
Notes: (Paved Shoulder)

1. For Roadway Section w/Guardrail, see Typ. Guardrail Section.
2. If existing condition in shoulder is Non-paved, see Typical Section B.
3. Clearing & Grubbing shall be provided for shoulder areas to be improved. The cost shall be Incidental to Paving Work.

* Where Exist. Slopes in areas beyond the Paved Shoulder are greater than 6:1, a maximum slope of 4:1 shall be used, as shown in Detail A, with approval of the Engineer.

** Mix V for this work shall be paid for under payment Item No. 401.0500.

*** Shoulder to match superelevation grade when superelevation is 5% or greater.

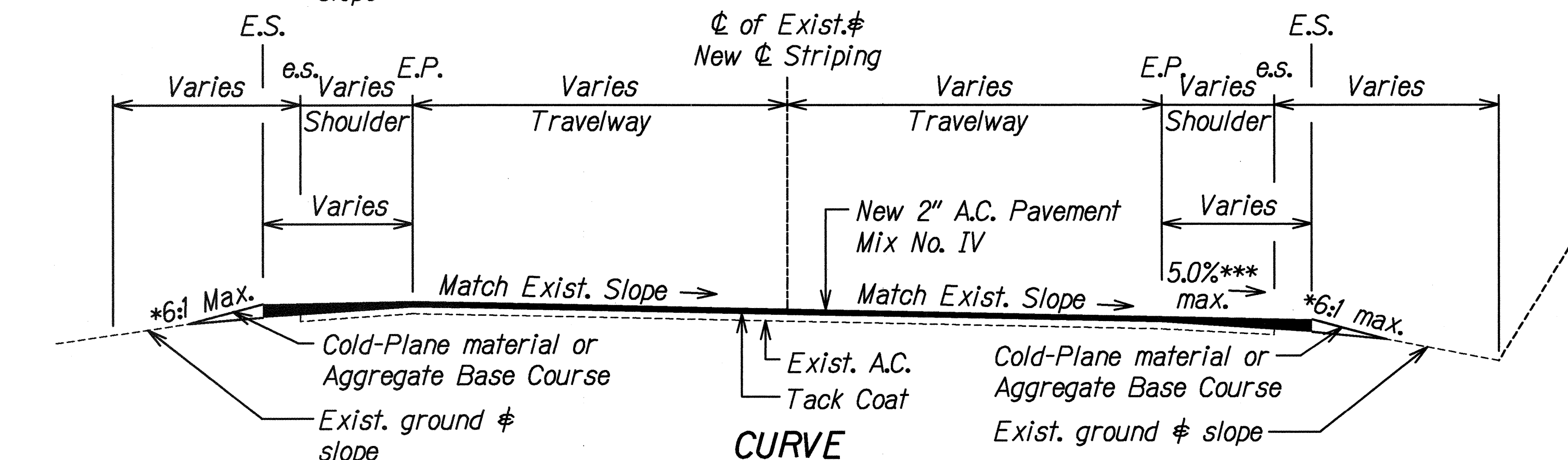
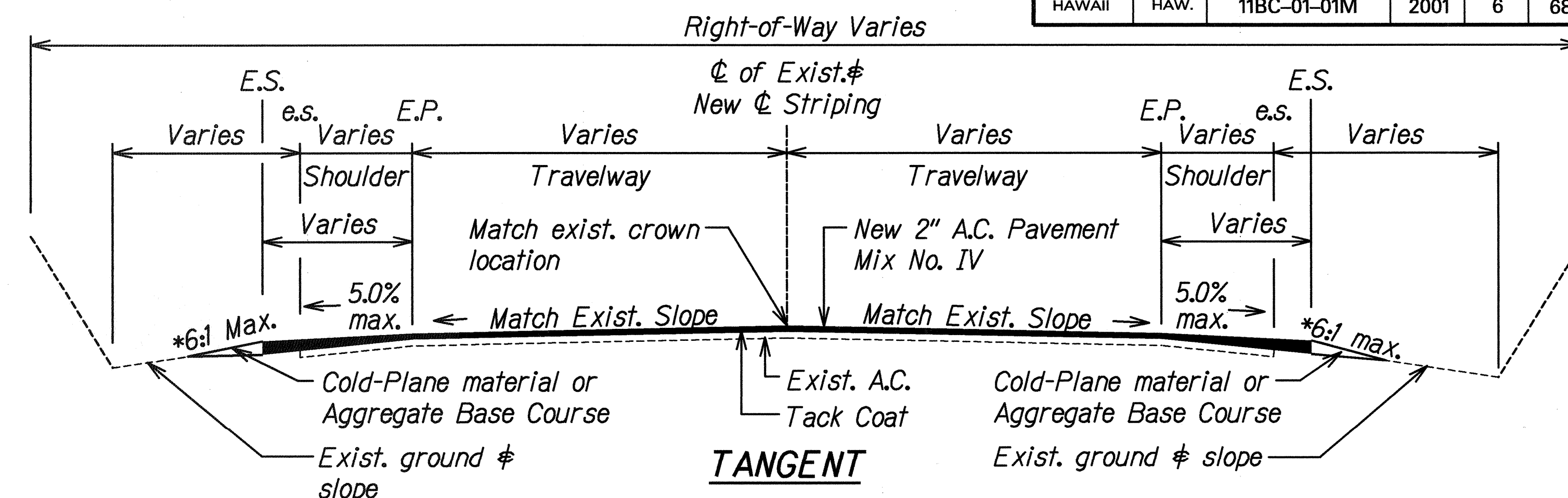


TYPICAL SECTION C FOR ADA

STA 389+89.20 Lt. to STA 483+72 Lt.

Scale: 3/8"=1'-0"

** Mix V for this work shall be paid for under payment Item No. 401.0500



TYPICAL SECTION B (Shoulder Partly Paved)

See Shts. 9 thru 11 for Locations of this Typical Section

Scale: 3/8"=1'-0"

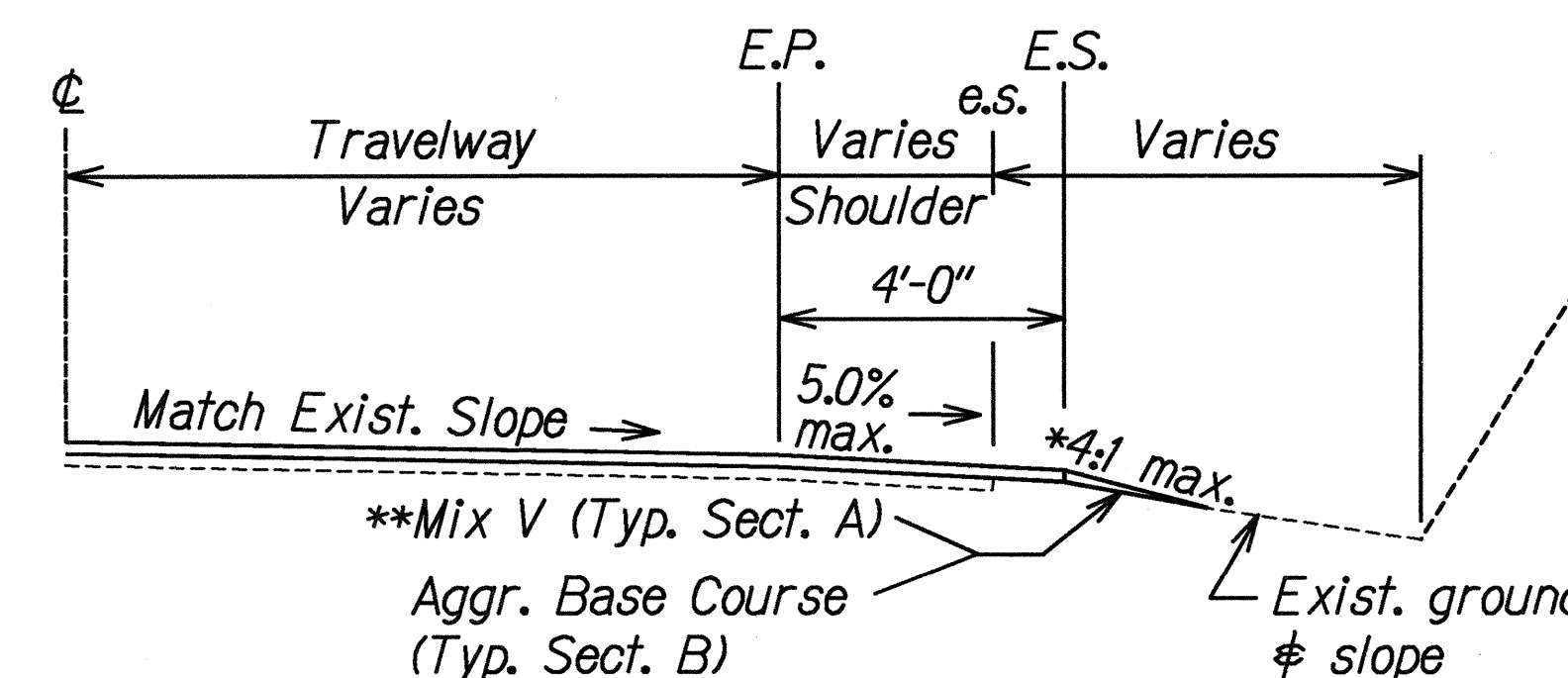
Notes: (Shoulder Partly Paved)

1. For Roadway Section w/Guardrail, see Typ. Guardrail Section.
2. If existing condition in shoulder is paved, see Typical Section A.
3. Clearing & Grubbing shall be provided for shoulder areas to be improved. The cost shall be Incidental to Paving Work.
4. Contractor shall utilize cold-planned material before using the Aggregate Base Course.
5. Typical Sections shall be centered using Φ and Φ striping re-alignment.

* Where Exist. Slopes in areas beyond the Paved Shoulder are greater than 6:1, a maximum slope of 4:1 shall be used, as shown in Detail A, with approval of the Engineer.

** Mix V for this work shall be paid for under payment Item No. 401.0500.

*** Shoulder to match superelevation grade when superelevation is 5% or greater.



DETAIL A

Scale: 3/8"=1'-0"



THIS WORK WAS PREPARED BY ME OR UNDER MY SUPERVISION AND CONSTRUCTION OF THIS PROJECT WILL BE UNDER MY OBSERVATION.
Masamiro Nishida
DATE: April 29, 2001

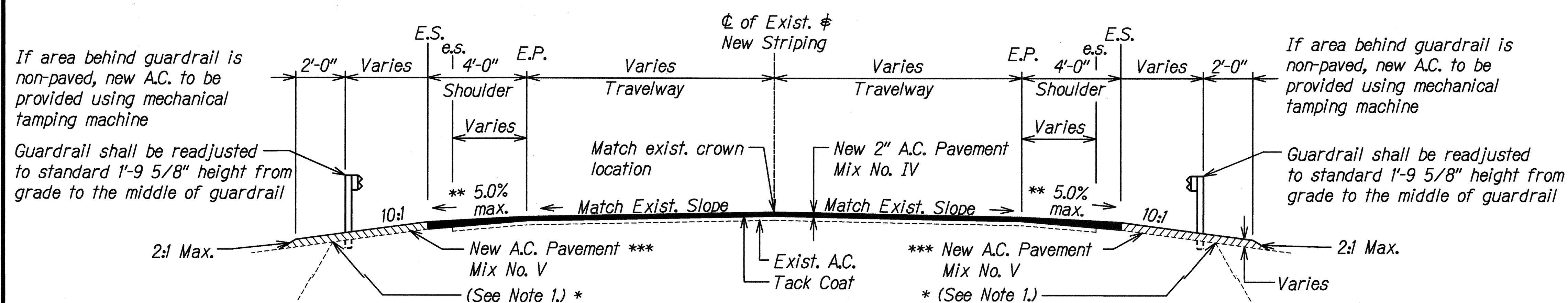
STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION
TYPICAL SECTIONS & DETAILS

**HAWAII BELT ROAD RESURFACING
HONOKUA TO PAHOEHOE
PROJECT NO. 11BC-01-01M**

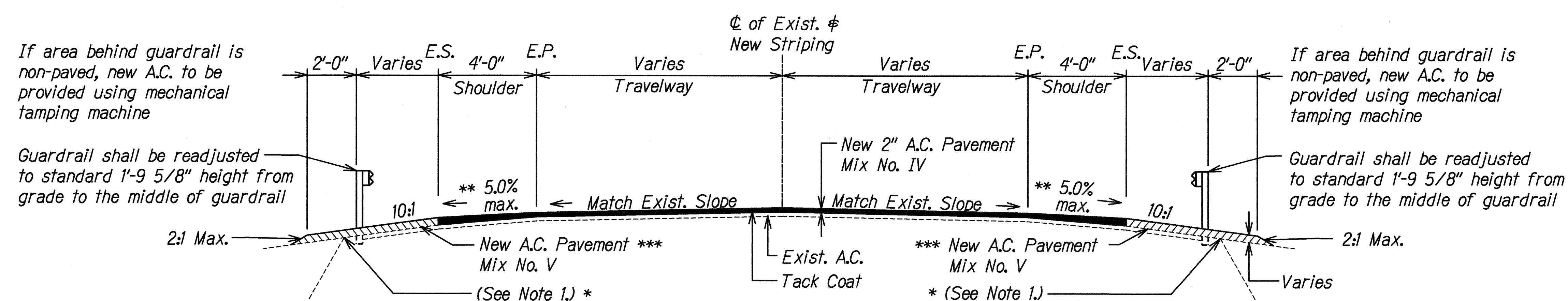
Scale: 3/8"=1'-0" Date: March, 2001

SHEET No. 1 OF 3 SHEETS

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	11BC-01-01M	2001	7	68



TYPICAL SECTION D NON-PAVED EXISTING CONDITION
Not To Scale



TYPICAL SECTION E PAVED EXISTING CONDITION
Not To Scale

- Notes: (Non-Paved and Paved Exist. Condition)
- Where the exist. guardrail does not have the minimum 2'-0" flat clearance behind the guardrail, paving shall end at the hinge of flat surface.
 - Clearing & grubbing shall be provided for shoulder areas to be improved. The cost shall be incidental to paving work.
 - Contractor shall dress shoulder drop-offs with usable cold plane material. Excess material shall be stockpiled at a location within the Project Limits determined by the Engineer at no cost to the State.
 - Typical Sections shall be centered using existing and new ϕ striping.
- * Mix V paving behind guardrails shall be paid for under payment Item 401.0550.
 ** Match existing slope at superelevation when 5% or greater.
 *** Item shall be paid for under payment Item No. 401.0500

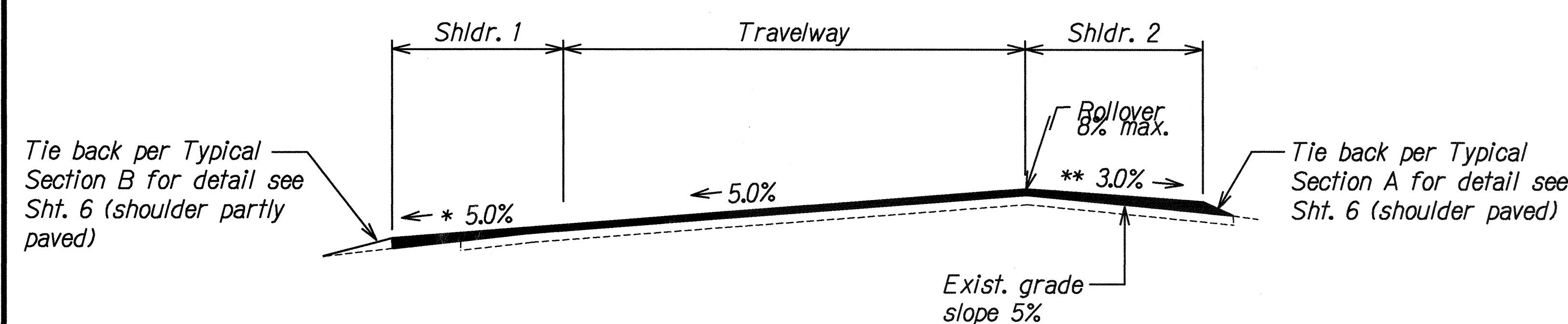


FIGURE - 1
Scale: N.T.S.

- NOTE:
- Roll over algebraic difference in rate of cross slope not to exceed 8% (See Figure - 1 Shoulder & for example).
- * Shoulder grade to match superelevation grade when superelevation is 5% or greater.
- ** Standard 5% shoulder to be used when superelevation is 3% or less on superelevations greater than 3% maintain maximum algebraic difference of 8%.

SURVEY PLOTTED BY	DATE
DRAWN BY	
TRACED BY	
NOTE BOOK	
CHECKED BY	
NO. 311-2001	

THIS WORK WAS PREPARED BY ME OR UNDER MY SUPERVISION AND CONSTRUCTION OF THIS PROJECT WILL BE UNDER MY OBSERVATION.

Signature
 DATE: March 29, 2001

STATE OF HAWAII

DEPARTMENT OF TRANSPORTATION

HIGHWAYS DIVISION

TYPICAL SECTIONS & DETAILS

HAWAII BELT ROAD RESURFACING

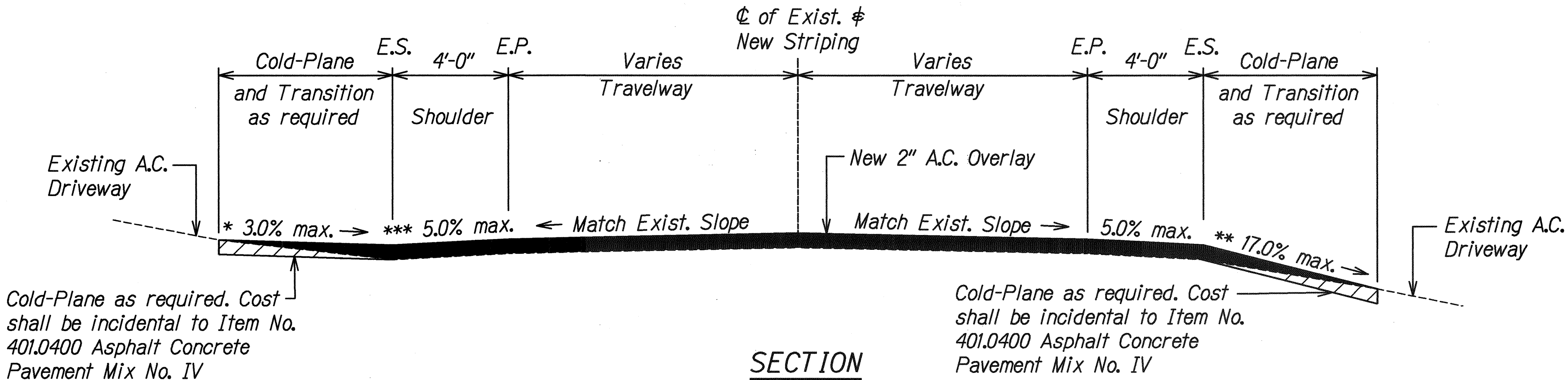
HONOKUA TO PAHOEHOE

PROJECT NO. 11BC-01-01M

Scale: 3/8"=1'-0"

Date: March, 2001

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAWAII	HAW.	11BC-01-01M	2001	8	68



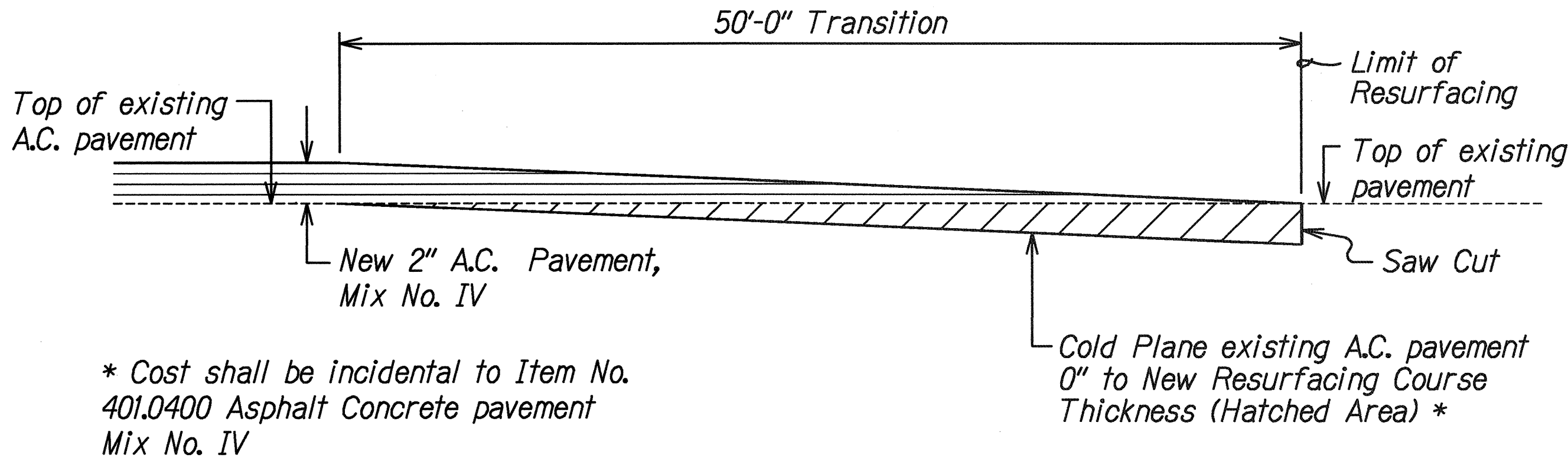
* Maximum algebraic grade break difference shall not exceed 8.0% on sag condition. Minimum tangent grade length to shoulder shall be 6'-0" when existing driveway grade is 3.0%. Contractor shall repeat grade breaks as required to meet existing driveways, with approval of the Engineer.

** Maximum algebraic grade break difference shall not exceed 12.0% on crest condition. Minimum tangent grade length to shoulder shall be 6'-0" when existing driveway grade is -17.0%. Contractor shall repeat grade breaks as required to meet existing driveways, with approval of the Engineer.

*** Sta 389+89.20 Lt. to Sta 483+72.00 Lt. Provide 5'-0" sidewalk with 2.0% cross slope at driveway connections. Refer to Typical Section C for sidewalk details see sht. 6.

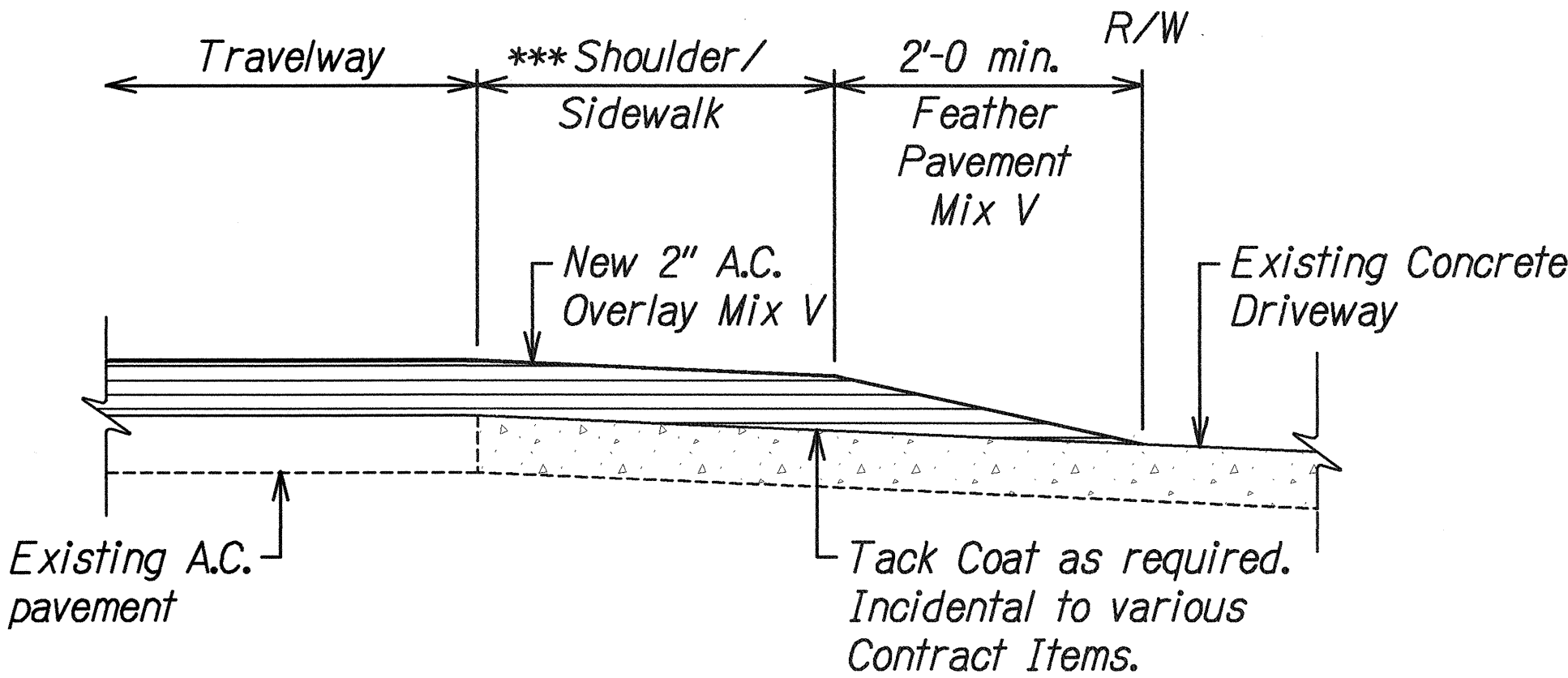
TYPICAL A.C. DRIVEWAY CONNECTION

Not To Scale



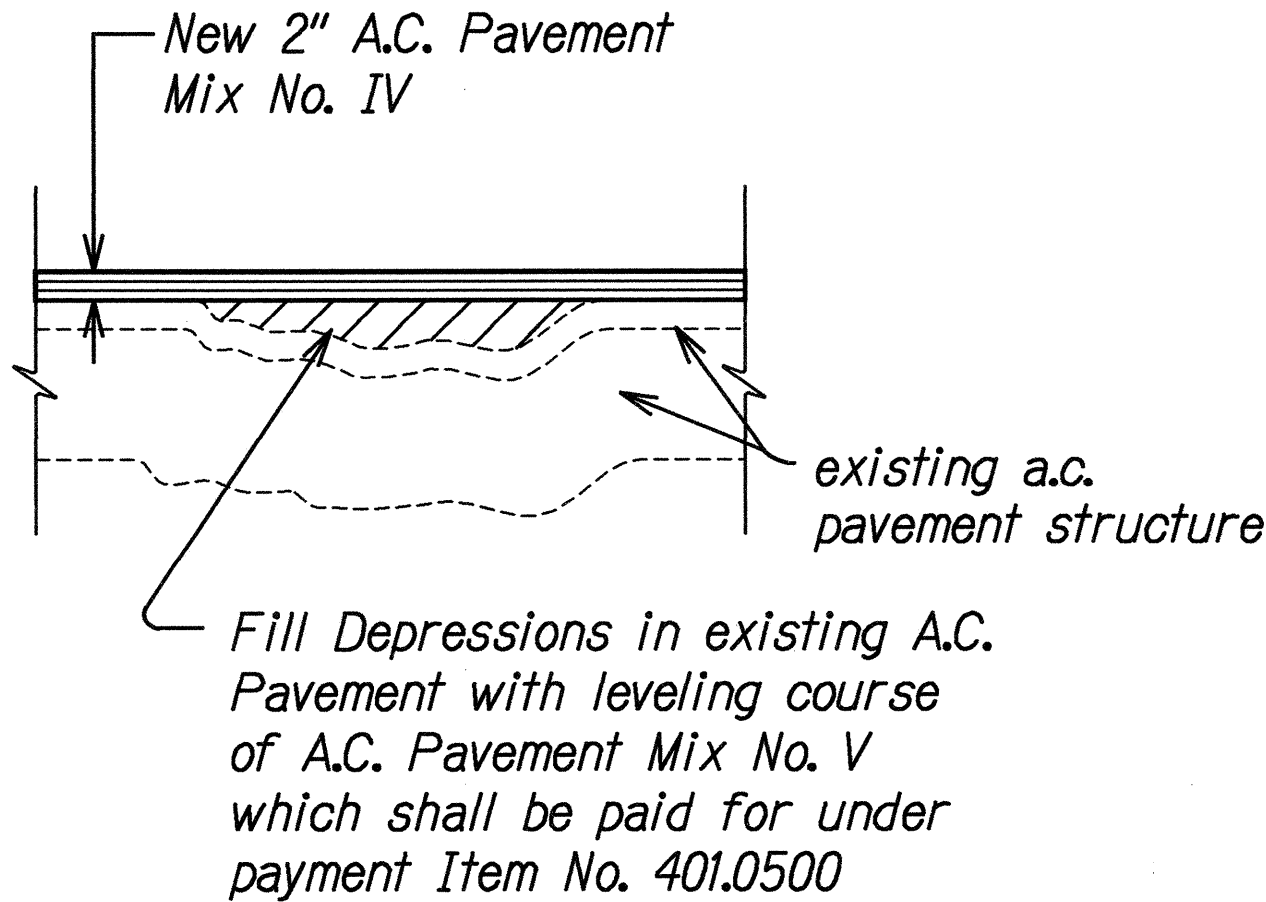
COLD PLANED TRANSITION TO EXISTING A.C. PAVEMENT

Not To Scale



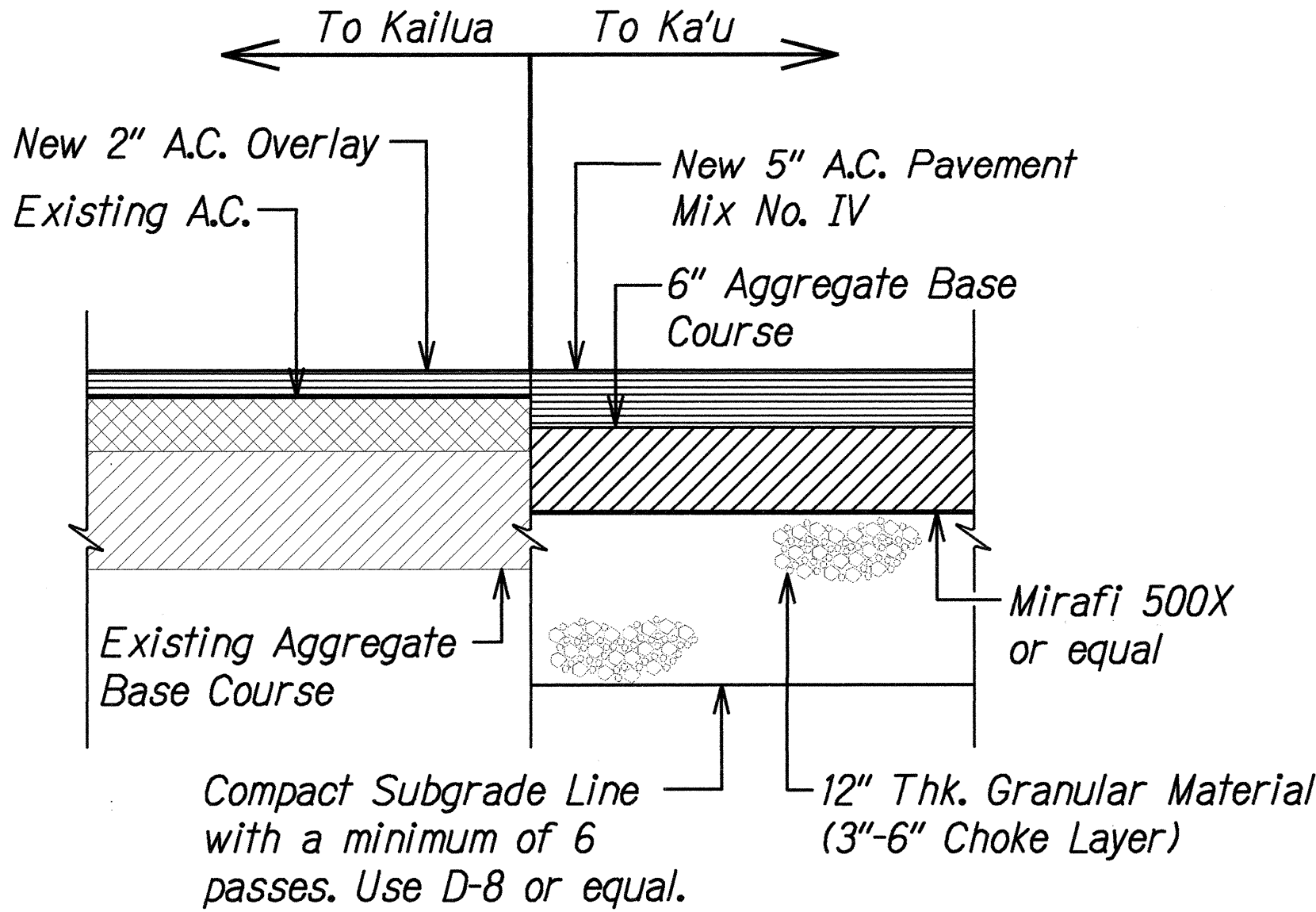
TYPICAL CONCRETE DRIVEWAY CONNECTION

Not To Scale



LEVELING COURSE DETAIL

Not To Scale



RECONSTRUCTION SPECIAL PAVEMENT AREA STA 563+17 to STA 563+97

See Sht. 19 for Location
Not To Scale



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DATE: March 29, 2001

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

TYPICAL SECTIONS &
DETAILS

HAWAII BELT ROAD RESURFACING
HONOKUA TO PAHOEHOE
PROJECT NO. 11BC-01-01M

Scale: N.T.S. Date: March, 2001

SHEET No. 3 OF 3 SHEETS

DATE	_____
SURVEY PLOTTED BY	_____
DRAWN BY	_____
TRACED BY	_____
NOTED BY	_____
CHECKED BY	_____
ORIGINAL PLAN	_____
NOTE BOOK	_____
NO. 5000	_____