

STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
HAW.	BMD 66-343	FY 71-72	2	88

GENERAL NOTES

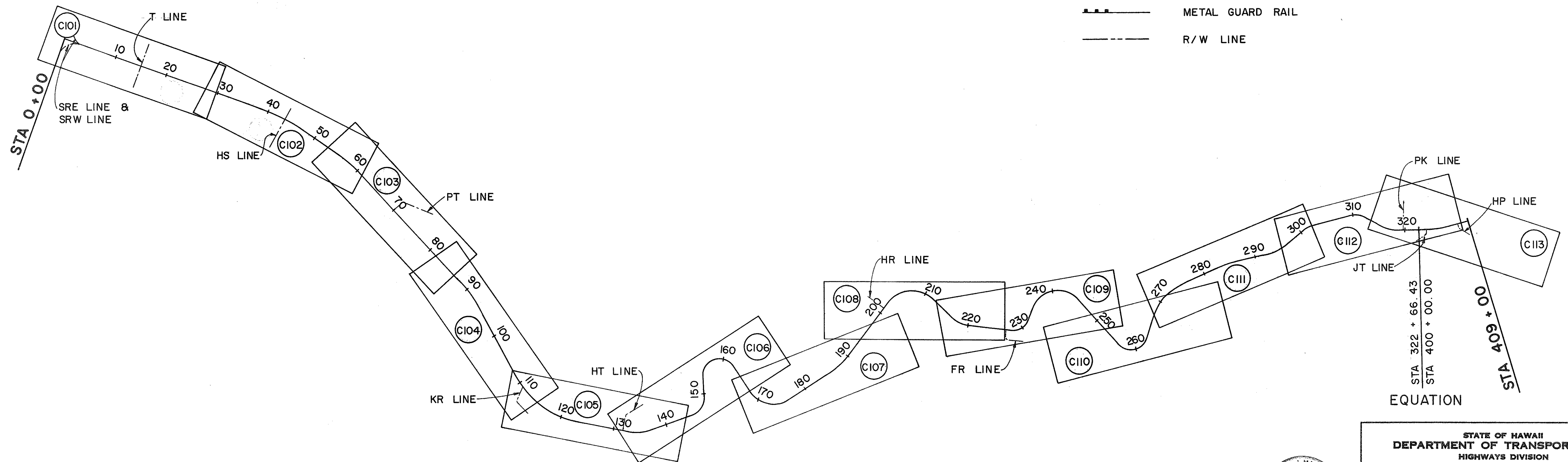
- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE STATE OF HAWAII, DEPARTMENT OF TRANSPORTATION, HIGHWAYS DIVISION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (1969) AND THE SPECIAL PROVISIONS.
- ELEVATIONS ARE BASED ON USGS MEAN SEA LEVEL DATUM.
- COORDINATES ARE HAWAIIAN PLANE COORDINATE SYSTEM, ZONE I.
- AZIMUTHS ARE ORIENTED FROM TRUE SOUTH.
- ACCESS SHALL BE MAINTAINED AT ALL TIMES FROM THE BEGINNING OF THE PROJECT AT THE SADDLE ROAD INTERSECTION TO HALE POHAKU NEAR THE END OF THE PROJECT. CONSTRUCTION WORK SHALL BE PERFORMED IN SUCH A MANNER THAT VEHICLES CAN PASS THROUGH OR AROUND WORK AREAS AT ANY TIME.
- DETOURS SHALL BE EITHER THE EXISTING ACCESS ROAD OR THE NEW ROAD SUBGRADE OR A GRADED AREA CONNECTING THE TWO AND SHALL HAVE A MAXIMUM GRADE OF 25%.
- DETOURS SHALL BE MAINTAINED IN A CONDITION AS APPROVED BY THE ENGINEER.
- ALL PLANS PREPARED BY THE CONTRACTOR FOR CONSTRUCTION PHASING, DETOURS AND TRAFFIC MAINTENANCE SHALL BE APPROVED BY THE ENGINEER PRIOR TO DETOURING TRAFFIC.
- CONSTRUCTION SIGNS SHALL BE PROVIDED AS DIRECTED.
- TRAFFIC MAINTENANCE, DETOURS AND CONSTRUCTION SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE SOIL INFORMATION SHOWN ON THESE PLANS IS FOR DESIGN PURPOSES AND FOR THE CONVENIENCE OF THE ENGINEER IN CONTROL OF EARTHWORK. THE DEPARTMENT ASSUMES NO RESPONSIBILITY WHATSOEVER IN RESPECT TO SUFFICIENCY OR ACCURACY OF THE INFORMATION OR OF THE INTERPRETATION THEREOF, AND WILL NOT IMPLIEDLY OR EXPRESSLY MAKE ANY GUARANTEE OF ANY OF THE SAME.

ABBREVIATIONS

AB	AGGREGATE BASE	PC	POINT OF CURVE
ASB	AGGREGATE SUBBASE	PI	POINT OF INTERSECTION
AC	ASPHALT CONCRETE	PIVC	POINT OF INTERSECTION OF VERTICAL CURVE TANGENTS
BM	BENCH MARK	POC	POINT ON CURVE
BVC	BEGIN VERTICAL CURVE	POT	POINT ON TANGENT
CL	CENTER LINE	POVC	POINT ON VERTICAL CURVE
CMP	CORRUGATED METAL PIPE	PT	POINT OF TANGENCY
CRM	CEMENT RUBBLE MASONRY	PVRC	POINT OF REVERSE VERTICAL CURVE
CULV	CULVERT	PVMT	PAVEMENT
Δ	INTERSECTION ANGLE	R	RADIUS OF CURVE
DWG(S)	DRAWING(S)	R/W	RIGHT - OF - WAY
ELEV	ELEVATION	RT	RIGHT
EP	EDGE OF PAVEMENT	SE	SUPERELEVATION RATE
EVC	END VERTICAL CURVE	SFP	STRUCTURAL PLATE PIPE
EW	EACH WAY	STA	STATION
EXIST	EXISTING	T	TANGENT DISTANCE
GRP	GRouted RUBBLE PAVING	TP	SOIL TEST PIT
INV	INVERT ELEVATION	TYP	TYPICAL
L	LENGTH OF CURVE	USGS	UNITED STATES GEOLOGICAL SURVEY
LT	LEFT	VC	VERTICAL CURVE
MAX	MAXIMUM	VERT	VERTICAL
MIN	MINIMUM	WC	WIDENING OF CURVE
MISC	MISCELLANEOUS		
NO(S)	NUMBER(S)		
NTS	NOT TO SCALE		
OC	ON CENTER(S)		

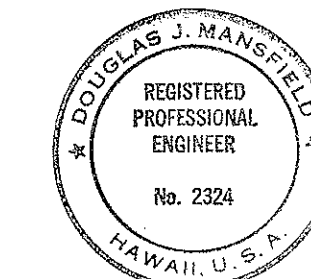
SYMBOLS

	EXISTING ROADS OR TRAILS		USGS MONUMENT
	INTERMITTENT STREAM		TRAVERSE CONTROL POINT 1/2" PIPE IN CONCRETE
	POWER LINE		USGS BENCH MARK
	TELEPHONE LINE		VERTICAL CONTROL POINT 1/2" PIPE IN CONCRETE
	POWER & TELEPHONE LINE		SOIL TEST PIT
	EXISTING UTILITY POLE		
	NEW UTILITY POLE		
	EXISTING FENCE		
	NEW FENCE		
	CATTLE GUARD		
	DITCH		
	FLOW		
	SIGN & POST		
	TOP OF CUT		
	TOE OF FILL		
	METAL GUARD RAIL		
	R/W LINE		



PLAN AND PROFILE SHEET LAYOUT INDEX

DATE	
DESIGNED BY	
TRACED BY	
DESIGNED BY	
QUANTITIES BY	
CHECKED BY	
NO.	



This work was prepared by me or under my supervision.

STATE OF HAWAII
DEPARTMENT OF TRANSPORTATION
HIGHWAYS DIVISION

**GENERAL NOTES, ABBREVIATIONS,
SYMBOLS & SHEET LAYOUT INDEX**

MAUNA KEA OBSERVATORY ACCESS ROAD
PROJ. NO. BMD 66 - 343
NO SCALE DATE: MARCH 1972

SHEET No. 1 OF 1 SHEETS 62